

Boronia

Structure Plan



Your Life, Your Place, Our Future

October 2006

Your Place, Your Life, Our Future

Boronia Structure Plan

Final Report
October 2006
Version 5

This report is available in large print & electronic formats
on request from Council's City Strategy Department



Knox City Council

Acknowledgements

The *'Boronia: Your Place, Your Life, Our Future'* structure plan has been prepared by the City of Knox in conjunction with the Boronia Community. The development of a plan for Boronia is the starting point of Council's Place Management Program for the centre. The Plan comprises a collective of contributions and recommendations from a range of groups, with particular appreciation to:

- the Boronia community, interest groups and key stakeholders who were actively involved, participated in public discussions, provided written submissions or who gave direction on specific aspects of the plan;
- members of the 24-person Boronia Project Reference Group;
- the Baird and Chandler Ward councillors;
- Council staff who participated in discussions and facilitated public consultation days. Particular appreciation is given to staff on the Boronia Project Team (Council's internal working group);
- officers of the State Government Department of Sustainability and Environment; and
- consultants involved in the preparation of supporting work:
 - Context Pty Ltd for the preparation of the *'Heritage Assessment'* of select sites;
 - Springboard Social Planning for the *'Boronia Social Environment Study'*;
 - Ratio Consultants for the *'Traffic and Transport Review'* and the *'Commercial and Retail Needs Assessment'*;
 - Access Audits Australia for the *'Boronia Access Review'*;
 - Stephen Foley & Associates (Quantity Surveyor) for select project costings;
 - Village Well for the facilitation of public consultation and visioning days;
 - Peter Edgely Pty Ltd for architectural illustrations and support; and
 - Tonya Hinde Design for the preparation of Colour & material schedules.



Background

What is the Boronia Structure Plan?

A structure plan is a plan for the future: a blueprint of improvements and developments which need to be undertaken in order to achieve a better future. The Boronia Structure Plan will provide a series of guiding principles, and a point of reference for the Council, the community and developers, when making decisions, planning and managing new developments. The structure plan provides guidelines for future building height, limits to building height where there were no previous limits, and where activity and development will be located.

Why is Council doing a Structure Plan?

Structure planning for Activity Centres aims to achieve the policies and objectives set out for 'Activity Centres' by the State Government in its *Melbourne 2030: Planning for Sustainable Growth* and to plan effectively to ensure changing community needs (e.g. including housing, retail needs and community facilities) are met.

The Boronia Structure Plan is being prepared, according to State Government directions, as a plan for the future of the Boronia Activity Centre. Boronia has been identified in the State Government's Metropolitan Strategy "Melbourne 2030" as a major Activity Centre and therefore Knox Council is required to develop a structure plan to direct any future development within the commercial area of Boronia and its immediate surrounds.

When compared to the guidelines for a Major Activity Centre under Melbourne 2030, Boronia does not fare too well. In order to reach a satisfactory standard to comply as an Activity Centre, the following aspects of the Boronia Activity centre need to be improved:

- the variety of land uses within the Activity Centre;
- the variety of housing types available;
- be better oriented towards non-motorised and public forms of transport; and
- provide more community gathering places and focal points within the centre.

Emerging Household Demands

A series of supporting studies and reviews have been conducted that underpin the strategic directions and intent of the Boronia Structure Plan. In addition existing Council policies and strategies have provided a point of reference. This includes the Knox Housing Statement, which seeks to accommodate additional housing needs for Knox over the next 25 years. Over the next 25 years, the City of Knox will need to accommodate an additional 15,000 households. As there are few vacant development sites remaining, this new housing will need to be encouraged within the existing suburbs and Activity Centres. To reduce the impact on the established residential neighborhoods, the Boronia Structure plan encourages new housing within the Activity Centre.

With specific reference to Boronia, the 'Boronia Social Environment Study' (September 2005) identified changes in Boronia's demographic and household characteristics. This study identified a 7.3% population increase within Boronia by 2015, which is the highest



projected growth for local areas in the City of Knox (average 1.9% over that same period). Alongside other social characteristics of the local area outlined in this document, of significance to future residential needs in Boronia is the increased need for 1-2 bedroom properties close to shops, services and transport, particularly to accommodate the growing 55 plus age group to allow existing residents to age in place.

Retail & Commercial Needs

Increasing the variety of stores within the centre, including bulky goods, homewares and ancillary stores is central recommendation within the *'Boronia Commercial and Retail Needs Assessment'* (March 2006) Ratio Consultants.

This will assist in attracting a wider section of Boronia's catchment by providing un-represented retail activities, and address the over provision of convenience retail premises within the centre.

The Ratio study further suggested addressing the disconnected nature (the centre operating as almost 3 separate convenience retail premises) as the most important aspect to address the future functionality of Boronia.



Table of Contents

1.0 Introduction	1
2.0 Boronia Structure Plan	4
3.0 Vision Statement	5
4.0 Strategic Directions	6
5.0 Structure Plan Framework	10
Pedestrian Space and Precincts Plan	
Land Use Framework	
Design Principles & Built Form (Building Height) Framework	
Accessibility Framework	
Social Well Being	
Art in Public Places	
6.0 Precinct Plans	39
Dorset Road	
Boronia Village	
Southern Terraces	
Boronia Junction	
Dorset Square	
Boronia Park	
7.0 Project Implementation and Priorities Plan.....	80



Figures

- Figure 1 – Regional Context
- Figure 2 – Aerial 2003
- Figure 3 – Boronia Project Study Area
- Figure 4 – Structure Plan Outline
- Figure 5 – Structure Plan: Pedestrian Space and Precincts Plan
- Figure 5(a) – Structure Plan: Pedestrian Space and Precincts Plan (Detail)
- Figure 6 – Structure Plan: Land Use Framework Plan
- Figure 6(a) – Structure Plan: Land Use Framework Plan (Detail)
- Figure 7 – Structure Plan: Built Form (Building Height) Framework
- Figure 8 – Accessibility Framework
- Figure 8(a) – Accessibility Framework (Detail)
- Figure 9 – Structure Plan: Precinct Plan
- Figure 10 – Precinct Plan 1: Dorset Road Environs
- Figure 11 – Precinct Plan 2: Boronia Village
- Figure 12 – Precinct Plan 3: Southern Terraces
- Figure 13 – Precinct Plan 4: Boronia Junction
- Figure 14 – Precinct Plan 5: Dorset Square (Option A) Lower Level
- Figure 14(a) – Precinct Plan 5: Dorset Square (Option A) Upper Level
- Figure 14(b) – Precinct Plan 5: Dorset Square (Option B) Upper Level
- Figure 15 – Precinct Plan 6: Boronia Park
- Figure 15(a) – Precinct Plan 6(a): Boronia Park (Option B) Erica Avenue Extension
- Figure 16 – Implementation Framework
- Figure 17 – Catalyst Project – Dorset Square South and Cinema Lane



1.0 Introduction

Your Place, Your Life, Our Future: Boronia

The Structure Plan catchphrase, 'Your Place, Your Life, Our Future: Boronia' was derived from suggestions from the Boronia community and the Boronia Reference Group.

The catchphrase draws on the fantastic distinct attributes and characteristics of place and lifestyle offered in Boronia. It acknowledges the many individuals that visit, and live and work in our community, and the role we can undertake to achieve our vision for Boronia.

1.1 Purpose of the Plan

The Boronia Structure Plan:

- comprises a collective vision established by Boronia's community and interest groups. The vision identifies a *future role* for Boronia and builds on highly valued characteristics of the local area, whilst maximising identified opportunities to achieve wider community benefit. The vision reflects a common understanding between the community, Council, state government and development sectors, of Boronia's desired future;
- provides a strategically based series of directions which will best meet the needs of the local community. Through this series of strategic directions and frameworks, future public and private works will be coordinated, through future planning, management and community building activities. These framework plans will guide the *integrated* development of the Boronia Activity Centre, ensuring that convenience and quality of environment and function are driving considerations in improving the liveability of Boronia for residents and visitors. The plan will direct the improvement of Boronia as a valued hub of community life, with an identity and social interaction;
- will be implemented through Council's Place Management Program, through a series of catalyst projects identified to facilitate the achievement of the vision and demonstrate a commitment to the continued improvement of the local area;
- will enable a triple bottom line approach to achieve a coordinated delivery of social, environmental and economic improvements in Boronia;
- seeks to implement the directives of *Melbourne 2030*¹, whilst ensuring future development is accommodated contextually. This is of particular importance

¹ The State Government's strategy to direct the future growth of Metropolitan Melbourne.

as Boronia has been identified as a major Activity Centre within this document, but the outstanding views it enjoys from select vantage points will limit the intensity of development within the centre. It is critical that the future intensification of activity and buildings does not impede on these views towards the Ranges.

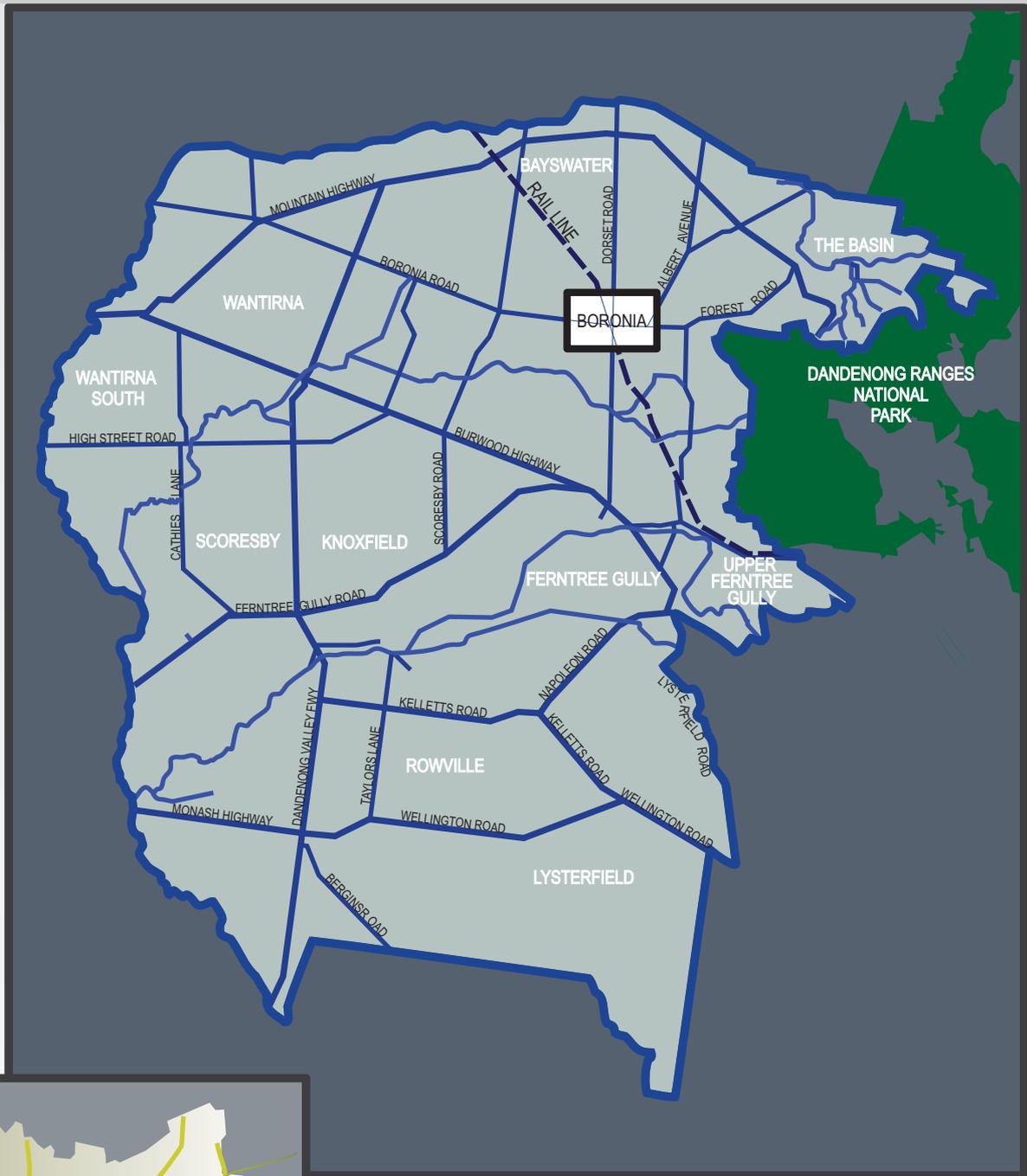


1.2 Defining the area of the Boronia Activity Centre

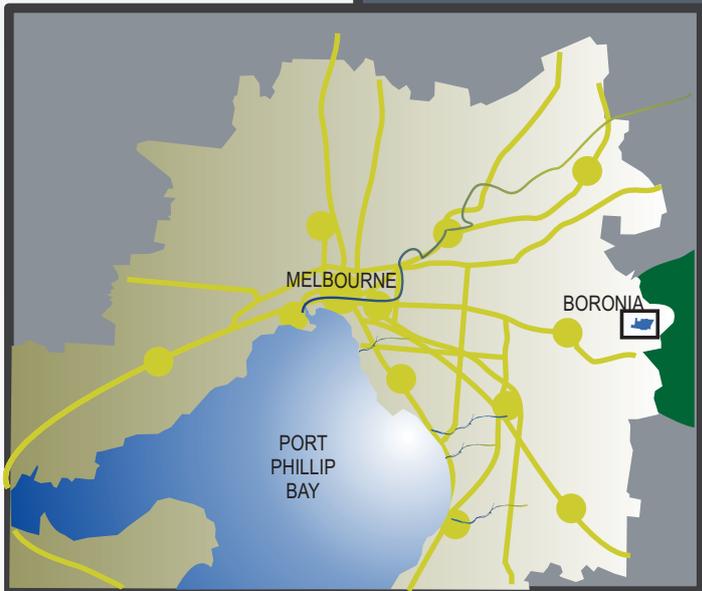
The structure plan study area (refer Figure 1) includes:

- the commercial and core retail uses;
- significant redevelopment sites including substantial VicTrack land and vacant/underutilised sites;
- significant attractors, points of interest, recreational and institutional facilities and schools within walking distance of the commercial environs; and
- a network of pedestrian, bicycle, vehicular and public transport routes.

The study area has been limited to ensure sufficient focus on the Activity Centre, and is loosely based on a 500m walkable distance from adjoining commercial areas.



CITY OF KNOX



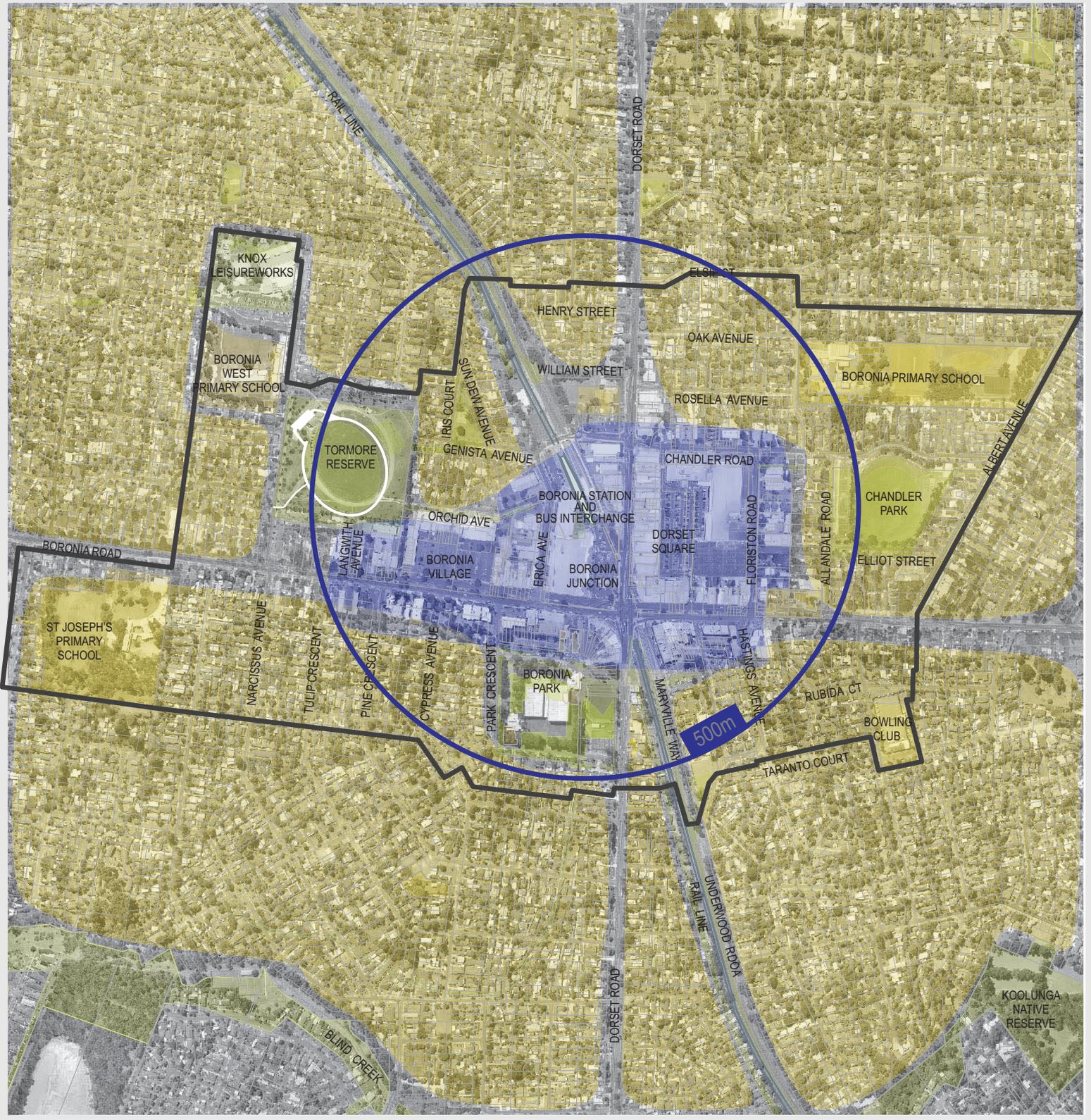
MELBOURNE METROPOLITAN REGION



STUDY AREA



Figure 2
boronia *Your Place, Your Life, Our Future*
Aerial 2003
(October 06)



- STUDY AREA
- OPEN SPACE
- EXISTING TREE
- RAILWAY LINE / RESERVATION
- SCHOOL / INSTITUTIONAL USE
- RESIDENTIAL
- BORONIA COMMERCIAL AREA



Knox City Council

Figure 3
boronia *Your Place, Your Life, Our Future*
 Study Area: Boronia Project
 (October 06)



2.0 Boronia Structure Plan

Your Place, Your Life, Our Future

The Boronia Structure Plan provides a physical framework for the future growth and development of Boronia. The structure plan comprises five sections:

- Vision Statement
- Strategic Directions
- Structure Plan Framework Plans
 1. Pedestrian Space and Precincts Plan
 2. Land Use Framework
 3. Design Principles & Built Form (Building Height) Framework
 4. Accessibility Framework
 5. Social Well Being
 6. Art in Public Places Framework
- Precinct Plans
 1. Dorset Road
 2. Boronia Village
 3. Southern Terraces
 4. Boronia Junction
 5. Dorset Square
 6. Boronia Park
- Implementation and Prioritisation Plan

Figure 4 – Structure Plan Outline – is a diagram illustrating the relationship between the five sections.



3.0 Vision Statement

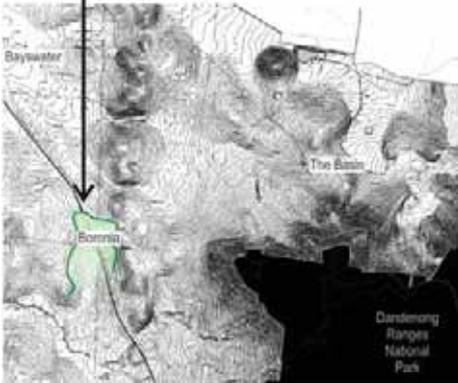
It is the Council and collective community's mission within Boronia:

"To create and sustain social well being, equity and commercial success within an environmentally sustainable framework."

In the future, Boronia will be:

- both a gateway to, and a destination at, the foothills of the Dandenong Ranges, providing a great place to live, a thriving hub of activity for commerce and all aspects of community life;
- a place that retains the distinct *nested* aspect at the foot of, and with views towards the Ranges;
- a safe pedestrian-focused centre providing ease of access for all users with different abilities and at all stages of life;
- a pedestrian-oriented centre that provides identifiable areas of interest, is well connected and easily accessible by walking, cycling, public transport and private vehicles;
- a place where public spaces are designed and developed using highly engaging and responsive processes, so as to represent the local community and environment;
- a centre which takes pride in its unique heritage including buildings representative of significant periods of Boronia's development;
- a series of diverse and confident retail and commercial precincts providing a vibrant and complimentary mix of businesses and services, and characterised by a unified trader and commercial community;
- a well maintained and clean environment incorporating leading edge ecological waste and litter management;
- a unique place attracting high quality development, urban design and streetscape improvements with sustained references to the unique foothills setting incorporating indigenous vegetation, species and exotic horticultural values that are a part of the local area;
- a place containing destinations, retailer and commercial premises, facilities, services, and amenities that provides ease of access for all users and abilities; and
- an environmentally sustainable centre incorporating best practice ecologically sustainable design principles.

Boronia is nestled at the foot of the Dandenong Ranges, and provides opportunities for quality lifestyle proximate to amenities, public transport, with a scenic outlook towards the National Park.



Vision

Strategic Directions

Structure Plan Framework Plans



Pedestrian Space Plan



Land Use Framework



Design Principles & Built Form (Building Height) Framework



Accessibility Framework

Social Wellbeing Framework

Public Art Framework

Precinct Plans

Dorset Road Precinct

1



Boronia Village

2



Southern Terraces

3



Boronia Junction

4



Dorset Square

5



Boronia Park

6



Implementation and Prioritisation Plan



4.0 Strategic Directions

The following strategies provide a foundation and point of reference for the Boronia Structure Plan.

Pedestrian Priority, Connectivity and Accessibility

- Compact the commercial area to improve walkability and accessibility.
- Strengthen and improve the level of amenity and safety of east-west and north-south pedestrian links across Dorset and Boronia Roads.
- Ensure a safe and continuously accessible path of travel for pedestrians of all abilities when designing public works and adjoining private development.
- Ensure access for all users and abilities is achieved, and the requirements of the Disability Discrimination Act are met in the design and management of new facilities, access into and within retail and commercial premises and amenities, including public toilets.
- Prioritise pedestrian safety and accessibility and add pedestrian crossings where suitable.
- Improve access between residential areas, the retail area and between recreational and community facilities.
- Provide a series of visually pleasing, highly landscaped and stimulating pedestrian environments, including public open spaces, commons, arcades, laneways and pathways.
- Design pathways where pedestrian movement and orientation is guided by visual cues, drawing on views and vistas towards the Ranges and between identified precincts.
- Provide a direct and clearly identifiable connection through commercial environs along the railway bike path.
- Increase walking as the preferred mode of travel into the centre.
- Provide new development facilities that encourage alternative forms of transportation including walking and bicycle use.
- Ensure the pedestrian environment, and pathways between areas of interest incorporate innovative approaches to lighting to improve perceptions of safety within an evening environment.

Land Use and Activity

- Establish Boronia as a vibrant and identifiable series of specialised commercial destinations, through the development of specialised precinct areas.
- Consolidate the commercial environs and core retail activities within Dorset Square and the Boronia Junction development, with active retail tenancies adjoining pedestrian pathways between these two areas.
- Facilitate growth in retail expenditure in Boronia by promoting the development of a bulky goods store and other homewares and ancillary stores, which will encourage linked trips to established core retail outlets.
- Seek a renaissance of Boronia Village as a mixed-use residential–commercial development, building on the views and vistas towards the Ranges.

- Provide an identifiable community hub or meeting place with direct access to the commercial environs where a variety of community uses and groups may be established in an environment that fosters life long learning.
- Clearly define edges to established residential environs.
- Provide locations where increased residential diversity will accommodate affordable and diverse housing forms in line with Boronia's current and emerging demographic characteristics, whilst maintaining a high level of architectural quality and design.
- Provide opportunities for residential and mixed-use activity within the commercial environs, whilst discouraging further fragmentation of commercially zoned land.

Sustainability

- Apply best practice ecologically sustainable principles in building and public space design through all stages of the design and construction process. Development must respond to local climatic conditions including wind mitigation and solar orientation, and improve the quality of the environment by reducing our impact on it.
- Incorporate best practice water sensitive urban design principles in the construction of both public and private sector projects.
- Ensure reduction of maintenance is a central consideration in the design and construction of public buildings and works.
- Preserve and augment the natural environment, areas of remnant vegetation and site's of biological significance within Boronia.
- Seek to retain established trees within the Activity Centre, in particular the commercial environment. Where trees are required to be removed provide an offset on site where possible, or in proximate areas.
- Build on the attributes of Boronia's unique setting at the foot of the Dandenong Ranges National Park and strengthen biodiversity corridors and links from the Ranges into the Activity Centre.
- Establish a community culture of reduce, reuse, recycle.
- Identify & undertake projects that unify the Boronia community on environmental aspects such as plastic bag reduction projects, waste wise businesses, community planting & clean up days.

Urban Design and Building Form

- Reinforce and build on Boronia as a meeting place and destination in the 'nest at the foot of the Dandenong Ranges.
- From critical vantage points maintain, and where appropriate, frame views from the west towards the extent of the Dandenong Ranges.
- Both broad area and detailed design of building heights and form should have a relationship to the topographical form and characteristics of the local area.
- Ensure that the colour and form of building materials reinforce the emerging contemporary natural character, including the naturalistic theme of the local area and the identified precinct areas.
- Build on the use of public and community art within the centre, and incorporate planting to emphasise links and help with direction finding.
- Acknowledge the significance of the Foothills Policy east of Dorset Road.

- Landscape within the Activity Centre should sustain references to the unique foothills setting incorporating indigenous species and exotic horticultural values that are a part of the local area,
- Vegetation, tree selection and soft and hard landscaping will consider the lifecycle, and ongoing maintenance appropriate in the context of an urban commercial setting.
- With the exception of business identification & directional signage, signage (including billboards) is to be discouraged within the Activity Centre, and along approaches into the centre.
- Reinforce the Bush Boulevard landscape character along road and rail approach routes to Boronia.

Heritage Context

- Identify and protect intact examples of early-built form fabric, including buildings (public and private) built during Boronia's significant periods of growth during the 1920s and the subsequent commercial development in the 1960s and 1970s.
- Protect places and heritage with distinctive architectural characteristics as well as those representative of post-war freestanding shopping centre development.
- Provide opportunities for the interpretation and celebration of the significant aspects of Boronia's growth and development.

Access, Traffic, Transport and Circulation

- Encourage alternative modes of transport to the Boronia Activity Centre by providing facilities and giving priority to pedestrians, bicycles and public transport.
- Enable access for users of all ages and abilities and different reasons for visiting, including work and deliveries.
- Adopt a rational approach to private vehicle access to Boronia and improve points of access, circulation, layout and efficiency of car parking facilities.
- Advocate for the continued improvement of public transport, including the spread of hours and coverage, frequency of service, safety and standard of facilities.
- Provide facilities, infrastructure and connectivity for bicycle access to, within and through Boronia.
- Ensure car parks & transport facilities are designed & maintained to achieve *Crime Prevention Through Environmental Design* Principles

Social Well Being

- Ensure Boronia is recognised as a great place for residents, workers and visitors.
- Improve social and leisure opportunities through quality design and 'inviting' environments for all members of the community.
- Seek opportunities to engage and involve the community, and encourage commitment, responsibility and ownership thereby increasing community pride and promotion.

-
- Build 'community connections'.
 - Support and develop talented leaders within the Boronia community.

Quality Environment & Maintenance

- Ensure a successful partnership to address the ongoing maintenance & cleanliness of the retail environment.
- Provide appropriate communal recycle, waste litter and cigarette disposal facilities throughout the commercial environment and at community and recreational facilities.
- Provide at source litter control systems including litter traps, a street / pavement cleaning system, drainage stencilling, plastic bag reduction programs, and building and construction maintenance to minimise litter impacts on the Blind Creek;
- Achieve an approach or model within the Boronia Place Management Program that integrates strategic asset management through all aspects of the project including:
 - the strategic planning and place management roles of project initiation;
 - project design and development through Council's landscape and place making teams;
 - design and construction;
 - handover to operations for ongoing maintenance; and
 - review of the assets over their lifecycle to best inform future works.

5.0 Structure Plan Framework

The Boronia Structure Plan comprises a series of broad plans outlining the preferred direction for the physical structure and coordinated improvement of the Boronia Activity Centre.

The Structure Plan comprises six component plans:

- Pedestrian Space and Precincts Plan
- Land Use Framework
- Design Principles & Built Form (Building Height) Framework
- Accessibility Framework
- Social Well Being
- Art in Public Places Framework

5.1 Pedestrian Space and Precincts Plan

(Refer to Figure 5 and 5 (a))

Principal Pedestrian Linkage

The central component in the structure plan from which many of the other supporting elements extend, is the Principal Pedestrian Linkage. The Principal Pedestrian Link is a 'back-of-road' or 'quiet street' connection, providing a continuous path of travel between areas of interest, where right of way for pedestrians is maximised. This link will provide a preferred alternate pedestrian link from the current links along the highly trafficked Dorset and Boronia Roads.

The Principal Pedestrian Link:

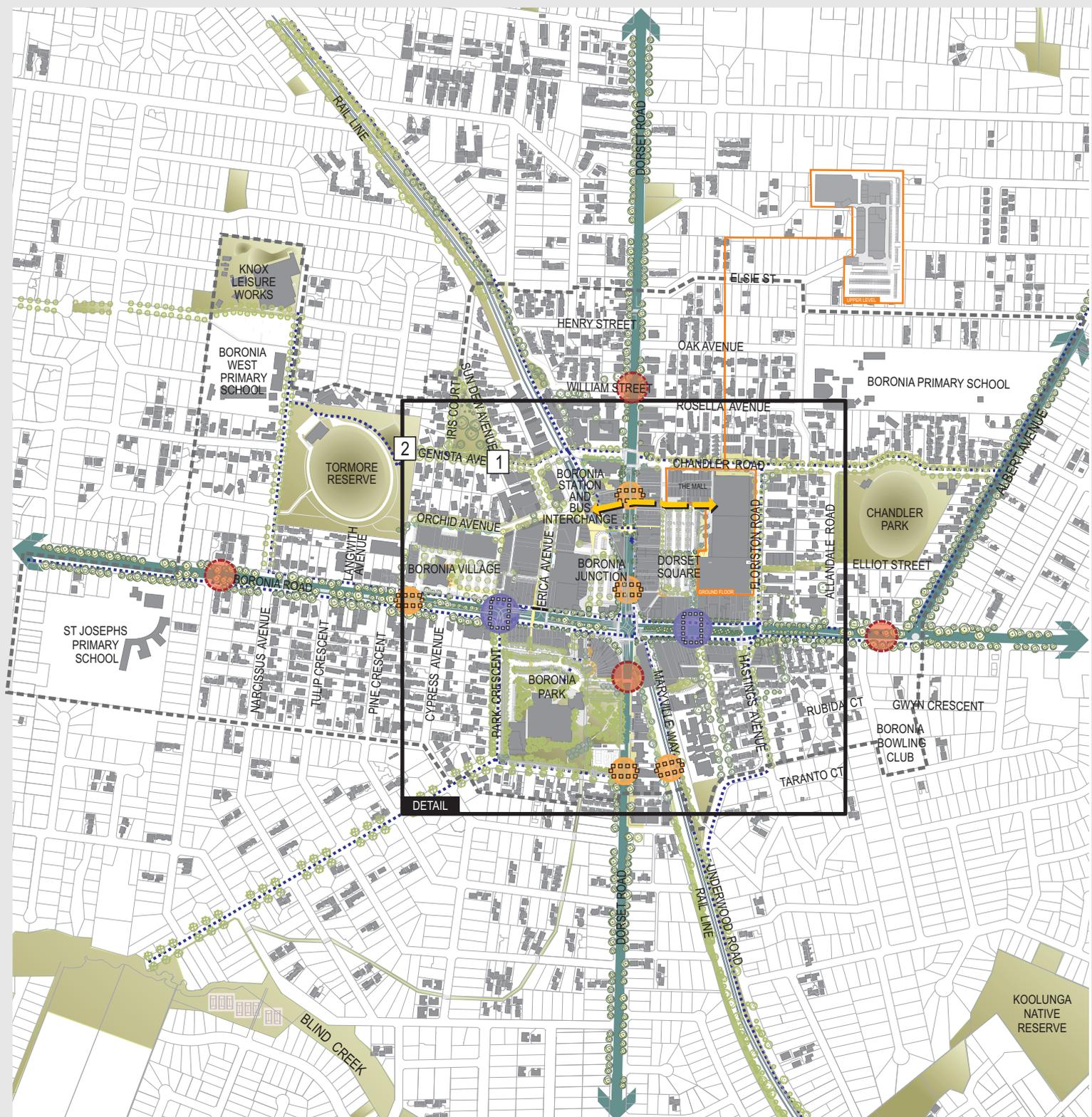
- will provide a safe, obvious and (through surface treatment) visually stimulating pedestrian connection between identified precincts and activity nodes, in particular:
 - Dorset Square and the station;
 - the station, Boronia Junction and Erica Avenue;
 - Boronia Park (and the community services and facilities node) to the commercial environs along Erica Avenue;
 - Dorset Square to the adjoining residential and commercial environs south of Boronia Road (the Southern Terraces);
 - Erica Avenue, the station and the Village.
- will provide ease of pedestrian access for users of all ages and abilities. This will include minimal changes in grade, sufficient length of time at pedestrian lights, pedestrian crossings to follow pedestrian-preferred lines, ample footpath widths, the use of tactile pavers and unobstructed pedestrian paths;
- will incorporate a widened pedestrian crossing east-west across Dorset Road, and provide opportunity for further review to determine the need for additional pedestrian lights on Dorset Road between Chandler and Boronia Roads;
- provides a pedestrian crossing at Boronia Road and Torrens/Turner Roads;
- will form a spine of activity through a series of promenades adjoined by highly active tenancies and buildings;
- will incorporate regular points of weather protection and will provide appropriate lighting, highly amenable pause points, seating, drinking fountains (including animal bowls) and bins;
- will be accessible at all times of the day and night;



The Principal Pedestrian Linkage will link defined activity nodes and areas of interest within the Boronia commercial area.



Rest areas & seating should be provided in areas of high activity along the principal pedestrian pathway.



- STUDY AREA
- OPEN SPACE
- PRINCIPAL PEDESTRIAN LINKAGE
- PEDESTRIAN SPACE SYSTEM
- INVESTIGATE FUTURE PEDESTRIAN CROSSING / ROUNDABOUT, INCLUDING PEDESTRIAN CROSSING
- INVESTIGATE FUTURE SIGNALIZED INTERSECTION
- CENTRE GATEWAY DESIGN TREATMENT
- BUILDING FOOTPRINT
- RAILWAY LINE / RESERVATION
- LANDSCAPE BUSH BOULEVARD APPROACH
- BIKE PATH
- UPPER LEVEL PEDESTRIAN CONNECTION
- 1 DISCOURAGE THROUGH TRAFFIC TO SCORESBY ROAD
- 2 PROVIDE LIGHTING ALONG PEDESTRIAN PATHWAYS IN TORMORE RESERVE



Figure 5

boronia *Your Place, Your Life, Our Future*

Structure Plan: Pedestrian Space

(October 06)



- will make reference & apply Council's footpath trading policy to ensure pedestrian clearways are maintained, to accommodate people with limited mobility or visual impairment;
- will incorporate directional signage including braille, walking time and distances to areas of interest including most proximate public toilet facilities;
- may be constructed in a distinctive surface treatment.

Pedestrian Space System

The wider Pedestrian Space System supports pedestrian permeability through the wider study area. It will continue to support walking as a preferred mode of travel to the centre.



The Pedestrian Space System will provide highly amenable pedestrian links between areas of interest within the wider Activity Centre.

The Pedestrian Space System will:

- establish and maintain a direct and continuously accessible path of travel through the centre to adjoining areas of interest and proximate residential environs;
- be designed to assist pedestrian movement and ensure their safety with appropriate surfaces and grade transitions (to facilitate crossings), lighting, passive rest areas (i.e. seating), the use of tactile pavers, and clear sight lines;

- be accompanied by a footpath trading policy to ensure pedestrian clearways are maintained, and to accommodate people with limited mobility or visual impairment;
- work with land owners to improve the attractiveness and amenity of the arcades, providing natural lighting, and active tenancies to improve perceptions of safety and security;
- require the investigation of additional pedestrian lights along Dorset and Boronia Roads to facilitate pedestrian connections between areas of interest;
- incorporate directional signage including braille, walking time and distances to areas of interest including most proximate public toilet facilities; and
- provide places for people to stop, contemplate and watch.

Green Links

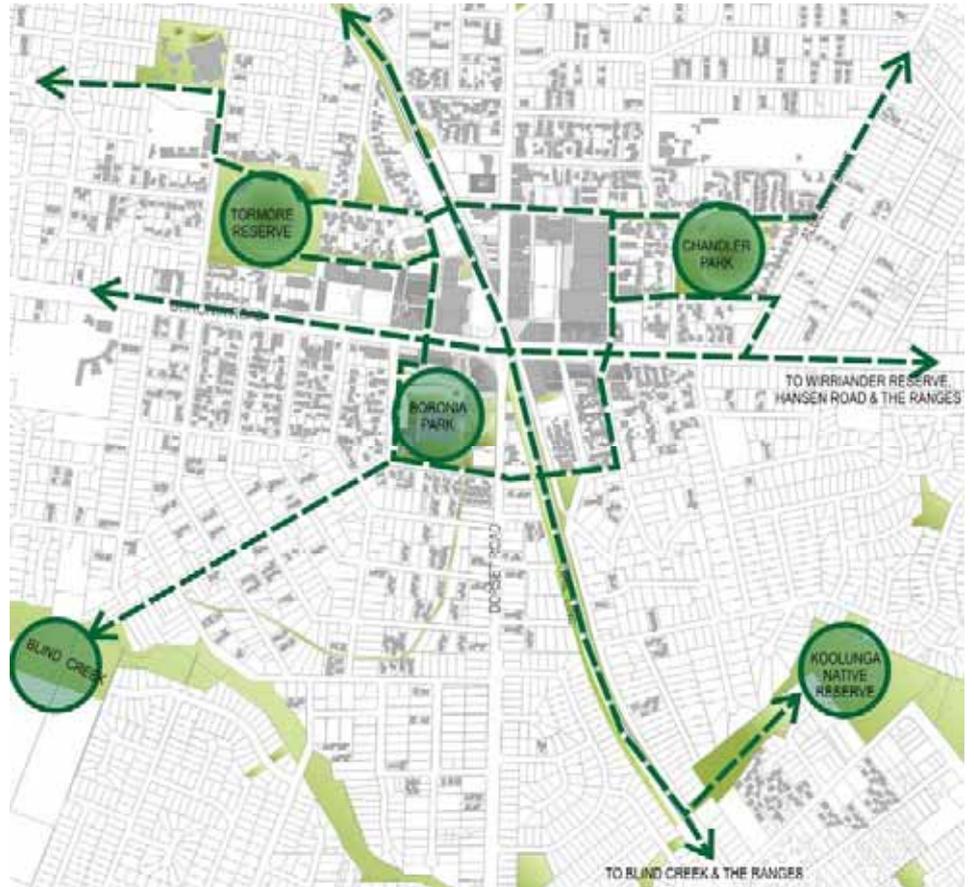
A series of green links will provide a web of treed pedestrian and bike paths between recreational nodes and areas of open space, and will have wider connections to the Dandenong Ranges National Park. These links will provide a landscaped path around and through the commercial environs to be used by visitors and workers in the centre. It will also provide connections between the commercial environs and:

- Knox Leisureworks
- Boronia West Primary School
- Tormore Reserve
- Chandler Park
- Boronia Park
- Blind Creek
- Koolunga Native Reserve
- proximate residential environments

Development adjoining the green links should contribute to the establishment of a substantially indigenous landscape setting through appropriate setbacks and landscaping and canopy tree planting. Active edges and/or buildings which address the street will create a sense of security and safety. In the public realm streetscape works should have a heavily landscaped quality, to calm traffic and give pedestrian priority. The paths will be signed and will include approximate walking times and distances. Entries to all parks & reserves along green links (and to adjoining residential areas e.g. Langwith Avenue, Orchid Avenue & Genista Avenue from Tormore Reserve) should seek to remove cyclone or like fencing, and should undertake landscape improvements at entrances. Where appropriate consider alternate opportunities for the seasonal or temporal closure of the entrances to enable the collection of gate revenue during sporting finals.



Green Links will provide a series of highly landscaped pathways between recreational nodes and areas of open space.



Centre Gateway Design Treatment

To define the point of arrival within Boronia, the points of entry along the Boronia and Dorset Road approaches will receive illuminated gateway treatment.

Although not within the commercial environs, the west, east and northern gateway points are aided by the topographical characteristics of the local area. These are locations where views into the commercial area can be seen from high points on the approach roads.

The southern gateway should reinforce and build on the presence and theme of the existing gateway treatment.

Gateway treatments may vary in form from an avenue of trees to banners, lighting, sculpture and signage or the like. Gateway treatments should build on the landscape theme that exists on approach routes.



Where a pathway crosses the boundary of the centre, Gateways can help identify & reinforce the entry to a local area (Example Western approach to Adelaide, Sir Donald Bradman Drive)

5.2 Land Use Framework

(Refer to Figure 6 and 6(a))

Within the study area, a series of preferred land uses has been identified for defined locations. The use categories described below, include both defined singular uses (e.g. residential), and more diverse mixed-use categories where more intense building form exists, or where transitions between existing commercial and established residential environs are anticipated.

Open Space

Boronia will continue to provide outstanding access to high quality parks, reserves and recreational facilities within the study area. These include:

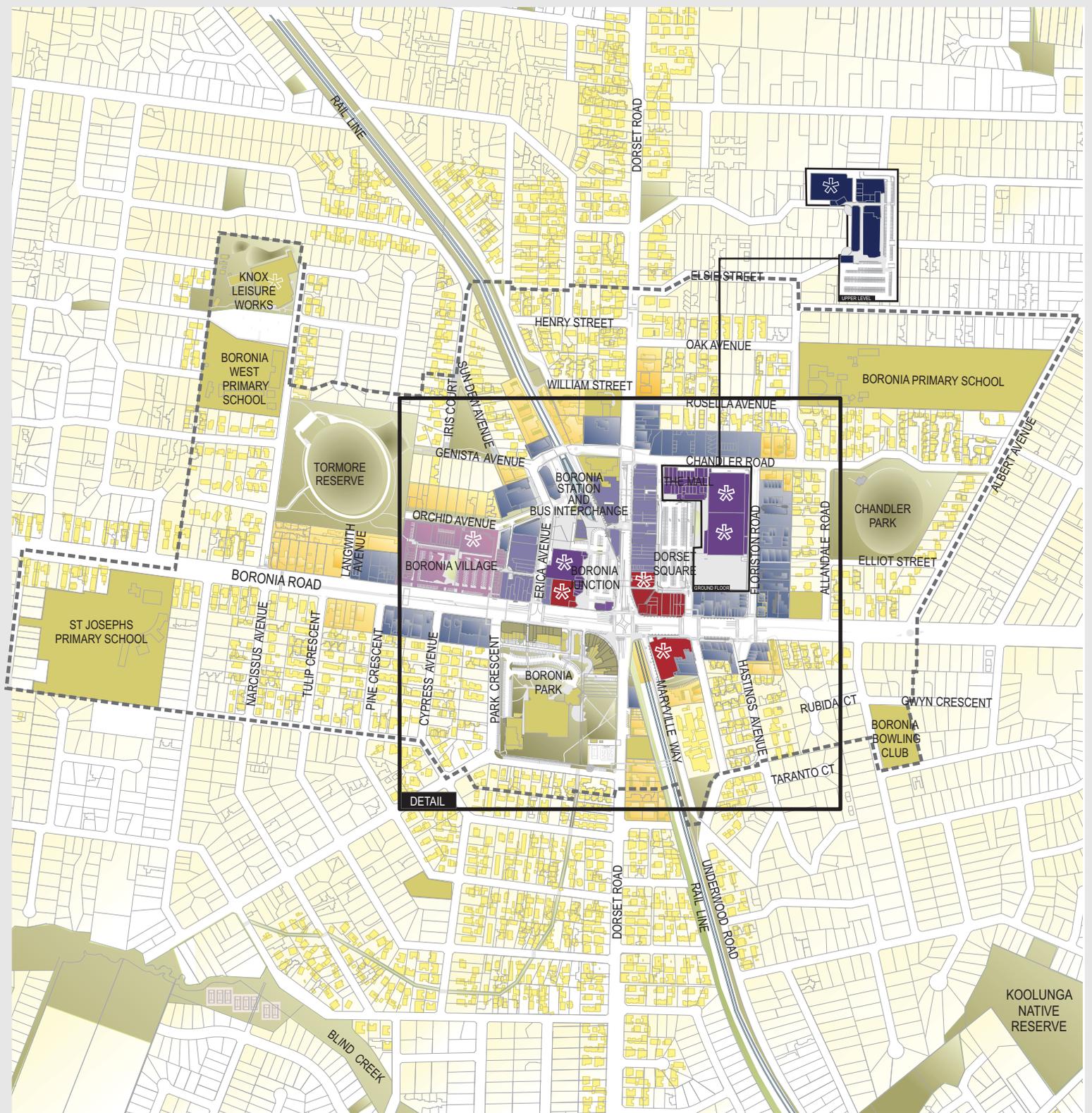
- Knox Leisureworks
- Tormore Reserve
- Chandler Park
- Boronia Park
- Blind Creek
- Koolunga Native Reserve
- Genista Reserve
- the railway bike path

The enhancement of these reserves will continue to support a range of passive and active recreational pursuits including community festivals and events which make the most of the outstanding views and proximity of the Ranges. Additional infrastructure, which could support a multiplicity of uses, may be considered on further review.

At entry points into reserves cyclone fencing should be removed and replaced with more appropriate treatment. This may include cleared vegetation and signage at entry areas. Pedestrian pathways through open space should be defined and appropriately landscaped particularly where they are providing a connection between areas of interest.

The immediate proximity of the Dandenong Ranges National Park (within 1.2km of the centre) provides a defined walking path linking the Koolunga Native Reserve and Blind Creek Pathways.

Future development adjoining reserves must significantly improve outlook towards the edges of reserves and must contribute to the landscape quality and improve perceptions of safety, access and amenity. The development should also allow for passive surveillance through low fencing, and by utilising materials and finishes reflective of the indigenous character of the local area.



- STUDY AREA
- OPEN SPACE
- ESTABLISHED RESIDENTIAL ENVIRONS
- DISPERSED INFILL RESIDENTIAL
- INCREASED RESIDENTIAL DENSITY
- RETAIL - COMMERCIAL ATTRACTOR
- CORE RETAIL
- BULKY GOODS RETAILING
- COMMERCIAL ENTERTAINMENT AND DINING
- MIXED-USE COMMERCIAL - PROFESSIONAL SERVICES
- VILLAGE MIXED-USE RETAIL - RESIDENTIAL PRECINCT
- COMMUNITY FACILITIES AND INSTITUTIONAL USES
- MIXED USE COMMUNITY SERVICES AND FACILITIES



Knox City Council

Figure 6

boronia *Your Place, Your Life, Our Future*

Structure Plan: Land Use Framework

(October 06)



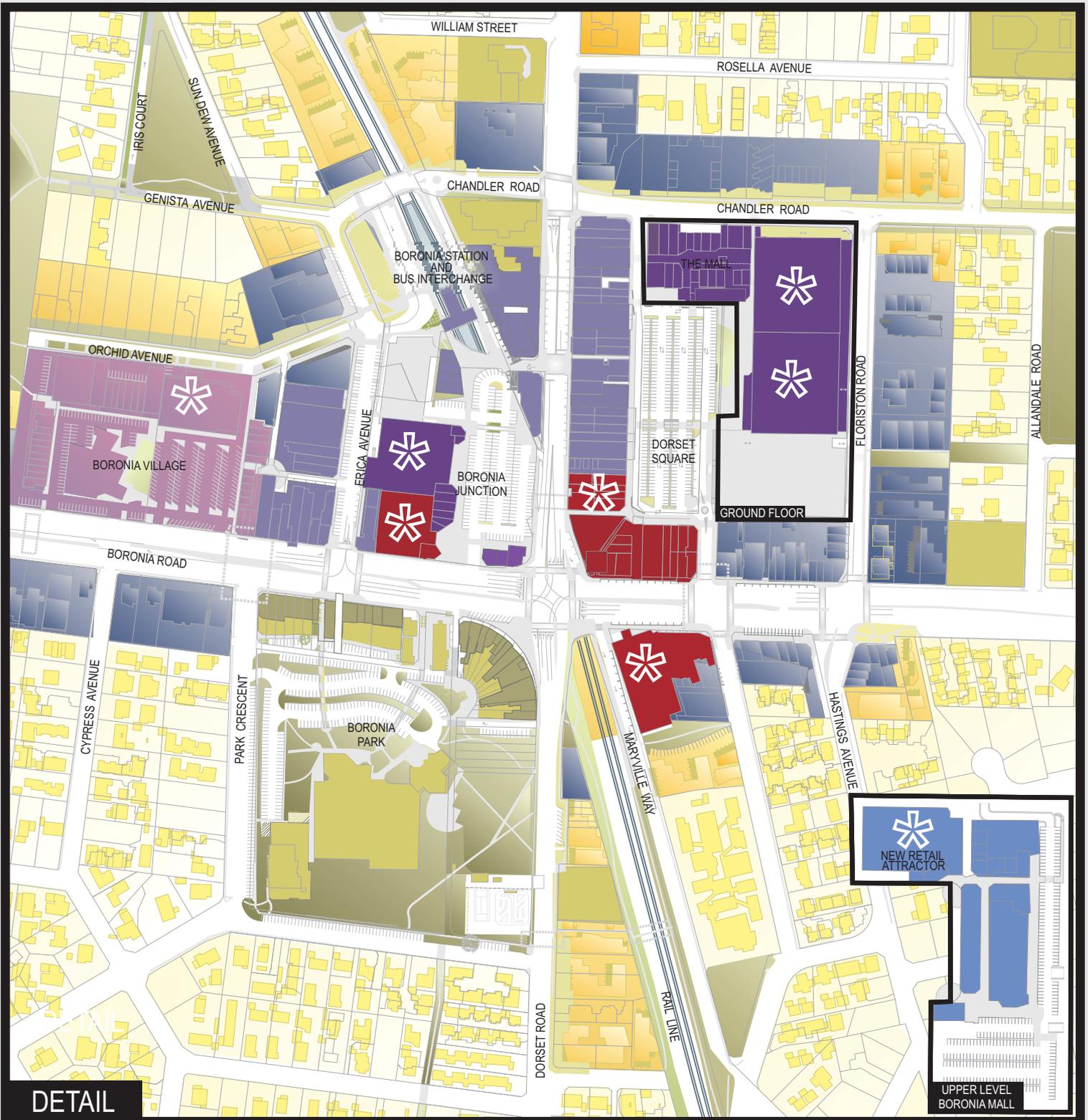


Figure 6(a)

boronia *Your Place, Your Life, Our Future*

Structure Plan: Land Use Framework

(October 06)



Established Residential Environs

Established residential environs include locations which are not within convenient walking distance of the Boronia commercial environs or the Principal Public Transport Network². These are locations where new infill development will provide a positive contribution to the preferred character of the local area. This will include areas in the Dandenong Foothills, even though housing opportunities there are limited due to broader landscape objectives.

Dispersed Infill Residential

Encourage increased³ residential densities and a greater diversity of housing types, sizes and affordability in areas which adjoin open spaces and reserves, and those within convenient walking distance and grade of the commercial environs and the Principal Public Transport Network. Within these environs the design of development should maintain the perception from the street of a single dwelling per lot.

Increased Residential Density

Limited areas within immediate proximity of the commercial environs and which provide a transition into adjoining lower scale 'Dispersed Infill Residential', are identified as locations to accommodate increased residential density development. These are locations where the building form of new development:

- will capitalise on the proximity of the commercial environs and public transport access;
- can achieve views to the Ranges;
- can borrow and build on the amenity of adjoining parks and reserves.

These are locations where alternate housing types can be provided for a diversity of groups and markets including an ageing population, youth and the 20-somethings or those looking for views of the Ranges without a backyard.

Increased residential density development must provide an outstanding level of architectural quality, incorporating materials and finishes reflective of the indigenous character of the local area, and must achieve an outstanding rating of ecologically sustainable design principles.

These locations may require a framework for site consolidation to increase residential development capacity, whilst achieving outstanding architectural and urban design quality.

² The 'Principal Public Transport Network' (PPTN) is nominated under Melbourne 2030 as a high quality public transport network that connects Principal and Major Activity Centres. It comprises the existing radial fixed-rail network (and extensions to this network) and new cross-town bus routes. Within the Boronia Project Study Area, the PPTN includes Dorset Road and the railway line.

³ The 'Boronia Social Environment Study' (September 2005) identified changes in Boronia's demographic and household characteristics. This study identified a 7.3% population increase within Boronia by 2015, which is the highest projected growth for local areas in the City of Knox (average 1.9% over that same period). Social characteristics of the local area indicate the increased need for 1-2 bedroom properties close to shops, services and transport, particularly to accommodate the growing 55 plus age group to allow existing residents to age in place.

Core Retail

The Core Retail environment encompasses much of the current core retail area including The Mall and Boronia Junction. Dorset Square generally functions well as it contains existing retail attractors including supermarkets and a department store. Core retail areas are to be complemented by Secondary Retail–Commercial Area, and connected by the Principal Pedestrian Linkage east–west across Dorset Road.

The strength of Dorset Square as a core retail environment will be further strengthened by the redevelopment of the upper level of The Mall into a Bulky Goods Retail precinct. This will provide opportunities for linked trips which will reinforce Dorset Square as Boronia's core retail area. It is envisaged the redevelopment will:

- consolidate the core commercial environs;
- increase economic benefit to local traders through increased patronage, particularly within Dorset Square.

Secondary Retail–Commercial Area

This includes the established west edge of Dorset Square, the adjacent Dorset Road commercial strip, the immediate environs around the station, and Erica Avenue. The revival of these areas should be focused where appropriate, around the Principal Pedestrian Linkage. Tenancies which are on the Principal Pedestrian Linkage between Dorset Square and Boronia Junction in particular, must incorporate active smaller retail tenancies which address the street/pathway. Inactive uses including offices, residences, community facilities or the like should be located on upper floor levels that overlook (through windows and balconies) the street/pathway.

A critical challenge for the Secondary Retail–Commercial area is the maintenance of a contiguous active retail edge from Dorset Road and Boronia Junction to Erica Avenue. Opportunities should be provided for active retail development or makeshift structures which allow on street activity (particularly around the station area).

Bulky Goods Retailing

The upper level extension of The Mall presents an opportunity for the establishment of an upper level bulky goods/home wear store retail attractor and may include ancillary café or similar facility which has a direct outlook over Dorset Square, with views towards the Lysterfield Hills. The development could be provided with direct vehicular access from the northern ring road (along Floriston and Chandler Roads)

Direct pedestrian access to the ground level of Dorset Square, via a lift, will allow visitors to patronise existing tenancies in Dorset Square thereby spreading economic benefit. The lift will support a direct pedestrian link to Floriston and Allandale Roads from the Dorset Square area.

Commercial, Entertainment and Dining

Boronia Road east of Dorset Road and the southern edge of Dorset Square (with links to the Bowls) will be designed as a mixed-use nighttime activity precinct. It is envisaged retail, commercial and hospitality services will be developed on the ground level, bringing complementary night time activity to the cinema complex, proximate bowling centre and the Zagame's complex. Upper floor levels may include a complimentary mix of uses including residential and commercial activity, designed to maintain a highly active focus on the square, and activities at ground level. Strategic tenancing of retail and commercial premises should be employed to facilitate the establishment of this precinct, as a place which is welcoming, and is perceived to be safe for all members of the local community, during the day and night. *Adult entertainment uses are discouraged from locating in this precinct as they are not welcoming for all members of the community and will not contribute to the objective of establishing this precinct as an inviting evening destination for all members of the local community.*

Landscape and urban design improvements both in the public and private realm should compliment the image of the precinct as an inviting evening destination for the local community. The laneway south of the cinema provides an ideal opportunity to improve perceptions of this precinct as an evening destination whilst providing a critical visual connection between the two evening attractors. Seek opportunities to represent youth within this precinct, through the establishment of a youth drop in centre at upper levels, and through the incorporation of youth artwork.

Encourage a link through the Chandler Arcade which provides 24-hour access between Dorset Square south and the south-east section of Boronia Road.

Mixed-use Commercial–Professional Services

Encourage the redevelopment and/or refurbishment of existing commercial premises, in particular premises with a floor area of less than 150 m². Encourage a mix of residential and commercial uses. Within the residential developments incorporate flexible ground floor tenancies adjacent to the street frontage, to enable home office or small commercial components with direct access to the street.

Along Boronia Road south of The Village (between Park and Pine Crescents) reinforce the established medical and health related services and provide a pedestrian link across Boronia Road to the Village.

Village Mixed-Use Retail–Residential Precinct

Encourage the consolidation of core retail activities east of Erica Avenue and explore the possibility of a mixed-use retail–residential development for Boronia Village. The redevelopment of Boronia Village and the upper level car park will see the renaissance of the Village as a vibrant commercial place, providing a hub/meeting place for the community with outstanding views to the Ranges.

This precinct will include a low-scale development in the upper level car park, which overlooks a common with views to the Ranges. Retail oriented tenancies, destination retailing and services should be accommodated at lower levels, with residential and select commercial (gymnasium/ office) development accommodated on upper levels.

Community Facilities and Institutional Uses

Numerous community facilities and institutional uses will be retained on existing sites. These include schools, churches, recreational facilities and clubs.

New community facilities and institutional uses will be collocated within the existing facilities node around Progress Hall. This area is to be highly visible, accessible and understood by the community as a precinct. Urban design improvement to the northern edge of Boronia Park will provide a highly amenable outlook for facilities with an address and access via the park. Active facilities and services will be encouraged at ground floor level, with less active uses (e.g. counselling services or meeting rooms) on upper levels.

The potential relocation of the Boronia Branch Library (refer to Figure 15) would provide a new building with frontage to both the park and Boronia Road, thereby increasing the presence of the community facilities precinct (to be known as the 'Community Hub'). The library facility at ground floor level may be complemented by upper level meeting rooms and facilities to accommodate life long learning in Boronia.

More frequent and direct pedestrian links north-south across Boronia Road will improve connectivity between the Boronia Park Community Facilities Precinct and adjoining commercial areas to the north.

Mixed-Use Community Services and Facilities

The Mixed-Use Community Services and Facilities Precinct includes upper level uses similar to the Community Facilities and Institutional Uses Precinct. It will provide a buffer zone, encouraging ground floor active retail and commercial services tenancies to build on the exposure to through traffic along the Dorset Road/Boronia Road frontage. Upper level development will include inactive community facilities (to be discouraged at ground floor levels), institutional uses and/or residential development overlooking the Boronia Park environs.

'Building Height' is defined as the maximum height of a buildings or works above the existing natural ground level of the site. This definition will encourage new development to follow the natural topography of site's within the context of the Dandenong Foothills.

5.3 Design Principles & Built Form (Building Height) Framework

Topographic and landscape characteristics of the study area and surrounds, coupled with the proximity of the Dandenong Ranges National Park, provide a series of outstanding views and outlooks from various locations within the study area. The Activity Centre of Boronia is situated within a 'nest' with a series of treed ridgelines which provide enclosure to the north, east and west.

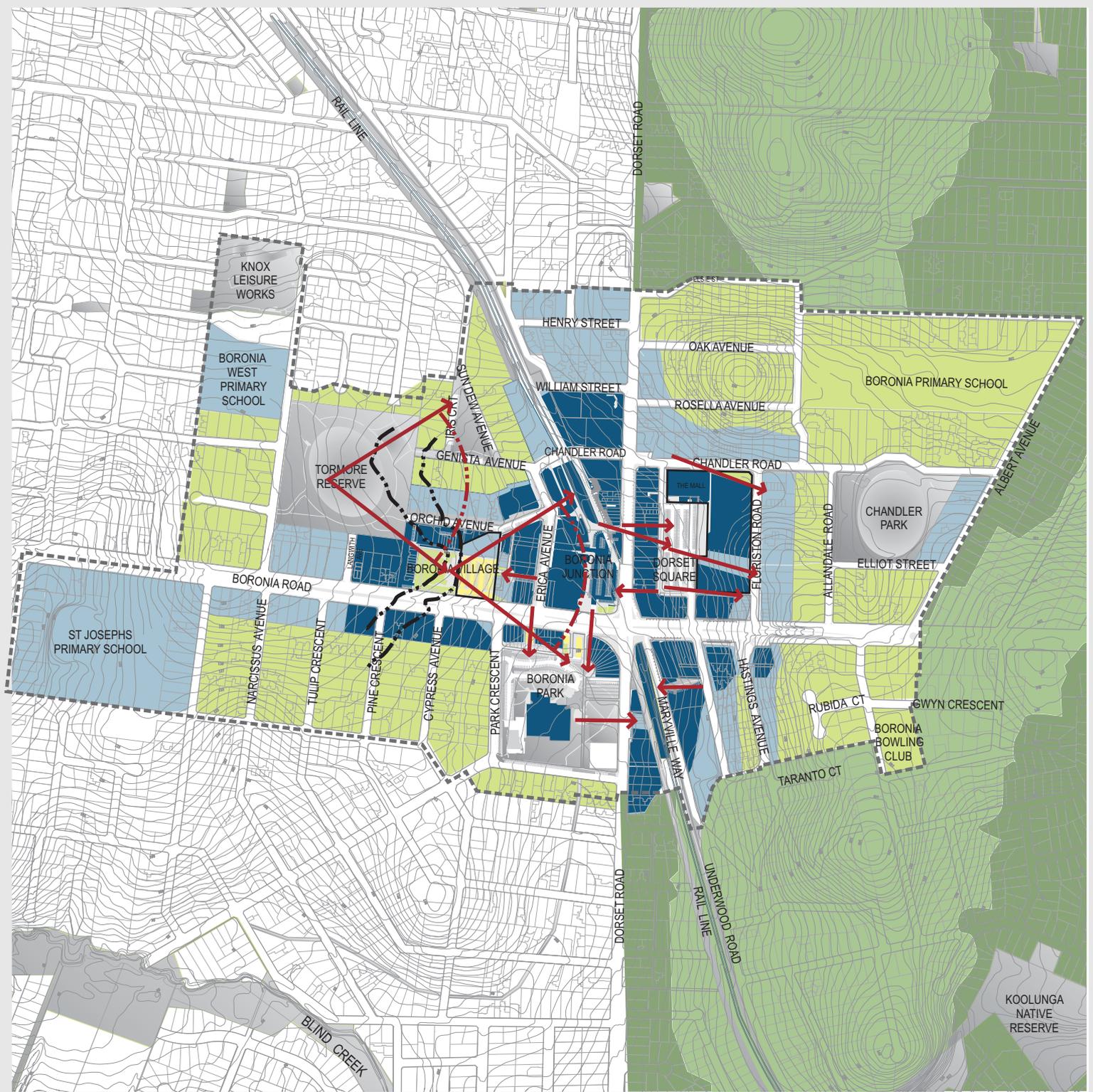
Consequently the commercial environs possess a series of outstanding topographically defined points of entry where views from the approaches to the centre extend towards more distant views, including south to the Lysterfield Hills, west towards Glen Waverly, and east towards the Dandenong Ranges National Park.

The most significant view lines within the commercial environs include those towards the east from the western section of Boronia. Of particular significance are views from Tormore Reserve and from Boronia Village. From these vantage points long Ranges views towards the Dandenong Ranges (almost in their entirety), provide an outstanding opportunity for outlook from new public spaces. Importantly, these views can help pedestrians find their way (by glimpses towards the Ranges or other landmarks) along defined pedestrian pathways.

Building Height

A Built Form (Building Height) Framework (refer to Figure 7) has been prepared for the extent of the Boronia Project Study Area. It provides a three-dimensional building form scale for Boronia defined by six key principles which have been derived from the visual experience, values and urban design structure of the Boronia Activity Centre, and will guide future development within the local area. These principles include:

- maintaining views towards the Dandenong Ranges National Park from current and future public places on the western ridgeline of the study area;
- providing a transition between more intense building heights within the commercial environs, and lower building forms in adjoining established residential areas;
- acknowledging the significance of the Foothills Policy;
- reinforcing opportunities for terraced forms of development in the south-east section of the centre, where a multiplicity of views can be shared by residents;
- increases in building height to be complemented by the establishment of substantial canopy planting of indigenous trees along nature strips and between buildings and the site's street frontage;
- developing underutilised land along the railway line where higher scale development can be built without direct streetscape, amenity or overshadowing concerns.



STUDY AREA

- 4.5 METRE BUILDING HEIGHT (1-STOREY)
- 7.5 METRE BUILDING HEIGHT (2-STOREY)
- 11 METRE BUILDING HEIGHT (3-STOREY)
- 14 METRE BUILDING HEIGHT (4-STOREY)

- FOOTHILLS AMENDMENT AREA**
- FOOTHILLS BACKDROP AND RIDGE AREA (2-STOREY/7 METRE BUILDING HEIGHT)
 - LOWER SLOPES AND VALLEY AREAS (2-STOREY/7 METRE BUILDING HEIGHT)

SMALL REDEVELOPMENT/BORONIA VILLAGE APPROXIMATE BUILDING HEIGHTS GIVEN ONLY. DETAILED DESIGN OF BUILDING SHOULD ENSURE VIEWS TO THE RANGES ARE MAINTAINED.

WESTERN RIDGE LINE: SIGNIFICANT VIEWS PROVIDED FROM TORMORE RESERVE AND BORONIA VILLAGE TOWARDS THE DANDENONG RANGES.

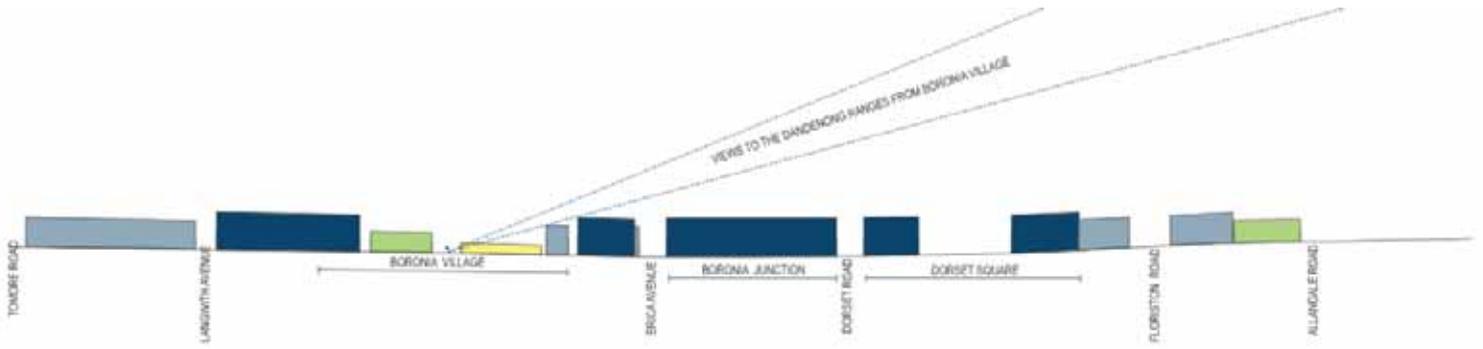
BUILDING HEIGHT PRINCIPLES

- ↓ - MAINTAIN VIEWS TO THE RANGE AND BETWEEN PRECINCTS WHILST ACCOMMODATING A MODERATE INCREASE IN BUILDING HEIGHT WITHIN THE COMMERCIAL ENVIRONS.
- REINFORCE A TERRACED FORM OF DEVELOPMENT - WITHIN THE SOUTH/EAST SECTION OF THE ACTIVITY CENTRE.
- PROVIDE A STEPPED TRANSITION TO ADJOINING 2-STOREY RESIDENTIAL AREAS.
- VISUALLY OFFSET INCREASE IN BUILDING HEIGHT THROUGH SUBSTANTIAL PLANTING OF INDIGENOUS CANOPY TREES ALONG NATURE STRIPS, BETWEEN BUILDINGS AND THE SITE'S STREET FRONTAGE.
- INCORPORATE THE INTENT OF THE FOOTHILLS POLICY, PROVIDING ONLY MINOR INCREASE IN RESIDENTIAL BUILDING HEIGHTS WITHIN WALKING DISTANCE OF THE COMMERCIAL ENVIRONS.



Figure 7
boronia *Your Place, Your Life, Our Future*
 Structure Plan: Built Form (Building Height) Framework
 (October 06)





SECTION HEIGHT INCREASED TO ILLUSTRATE CHANGES IN TOPOGRAPHY, BUILDING HEIGHT AND VIEWS TO THE RANGES.

As outlined in the above diagrammatic cross section through the commercial environs, current and future public places on the western ridgeline of the study area will maintain views towards the Dandenong Ranges.

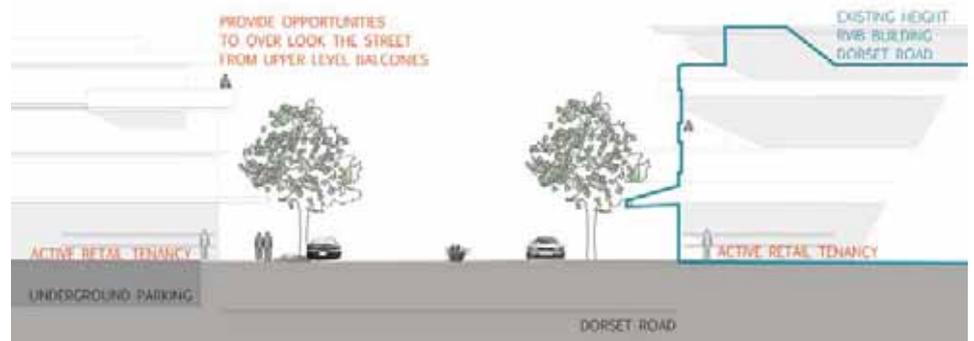


Architectural Quality and Design

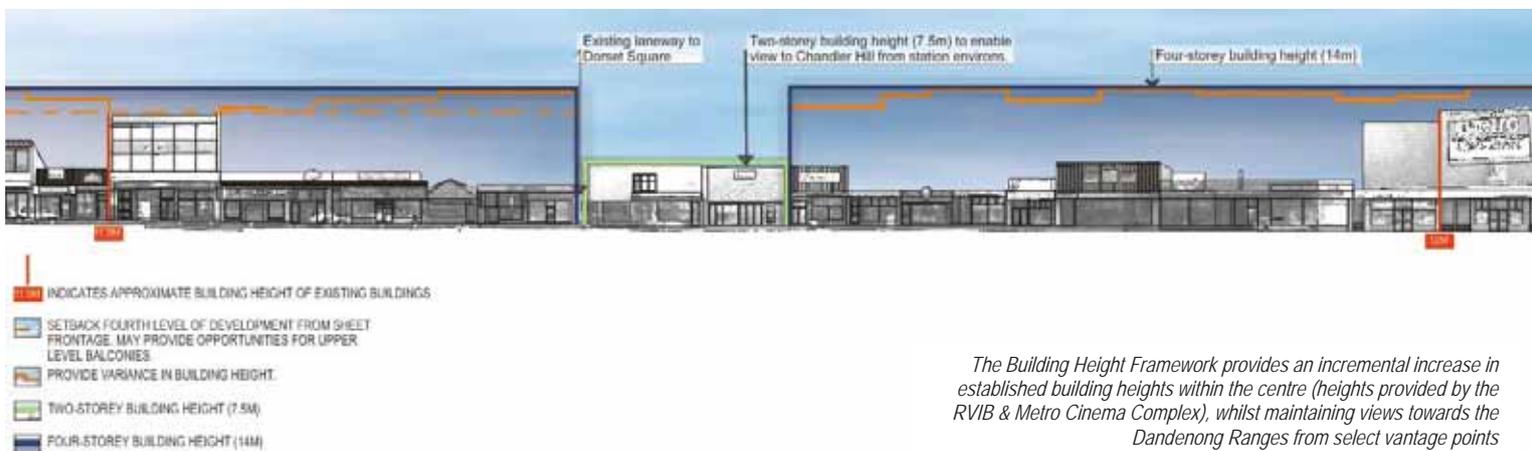
Within the context of the above building height recommendations, new development and refurbishments within Boronia must achieve a high standard of architectural quality and design and should contribute to the emerging contemporary indigenous character of Boronia.

New development and refurbishments must:

- provide reference to the topographic indigenous theme of the local area;
- ensure the interface between the public realm and new development has regard to the human scale and perceived safety of these environments through active edges and opportunities for passive surveillance;



- ensure roofing materials minimise glare and reflection as roof forms are highly visible from numerous vantage points within Boronia and from the adjoining National Park;
- minimise the visibility and reflectivity (from the adjoining National Park) of development within the Activity Centre by incorporating recessive building colours and materials for upper level development;
- incorporate vertical planting to building facades where possible;
- apply architectural techniques, including contrast, colour, texture and variation of materials, to detail buildings and provide a level of visual interest;
- incorporate artwork along blank walls or to screen car park areas, where they meet the Principal Pedestrian Linkage, Pedestrian Space System or Green Links;
- highlight key corners and points of entry into the centre through projected building forms where possible;
- maximise opportunities to strengthen landscape themes and incorporate substantial canopy planting in new development and the public realm;
- reduce visual clutter within the centre particularly that created by excessive visual signage;
- protect and provide reference to existing heritage structures and fabric;



The Building Height Framework provides an incremental increase in established building heights within the centre (heights provided by the RVIB & Metro Cinema Complex), whilst maintaining views towards the Dandenong Ranges from select vantage points

- where development adjoins established residential environs, provide a demonstrated transition between the building height of new development and the height of existing residential environs;
- address privacy concerns of established residential environments in proximity of new development. This may include the use of screens, obscured glazing and vegetation; and

-
- ensure new development does not unduly overshadow habitable room windows and private open space areas of existing adjoining residences.

Sustainable Design

The physical setting of Boronia nestled at the base of the Dandenong Ranges National Park, provides an imperative for the incorporation of sustainable design principles within new development and works in centre. Council, through the delivery of capital works, and (where opportunities arise through demonstration projects) will seek to deliver projects that demonstrate best practice examples of sustainable design for the community and development industry.

New development, building refurbishments and works must exercise principles of ecologically sustainable design including:-

- solar orientation: Maximising northerly aspect and solar access in the cooler months, insulate, double glaze windows to the south, incorporate operable sun shading devices, and minimise the westerly aspect of development;
- orientating development and openable windows for cross / natural ventilation;
- incorporating solar hot water heating;
- reduced energy usage through the installation of energy efficient appliances and lighting;
- recycling buildings and ensuring buildings are designed and constructed to adapt to accommodate a range of uses over time;
- using recycled building materials, and locally sourced products where possible;
- water management: reduced potable water use through fittings, installation of tanks for rainwater harvesting, maximise water recycling opportunities (and use of grey water) and treating storm water prior to discharge into waterways;
- maximise the use of indigenous vegetation to improve biodiversity corridors and links to the National Park, and minimise the need for watering; and
- improve the provision of shade, particularly along hard standing areas to reduce heat retention in the urban setting.

Heritage

Boronia has a number of rare, intact examples of early-built form fabric, including buildings (public and private) built during Boronia's significant periods of growth during the 1920s and the subsequent commercial development in the 1960s and 1970s. Future development within the centre should:

- protect places and heritage with distinctive architectural characteristics including those representative of post-war freestanding shopping centre development;
- provide opportunities for the interpretation and celebration of the significant aspects of Boronia's growth and development through festivals and displays.

A number of buildings require further investigation to determine their heritage significance. These include:

- the former Safeways building (159 Boronia Road);
- Boronia Mall tile mosaic/ mural (corner Chandler and Floriston Road);
- the series of californian bungalow style buildings (8, 10, 12 Park Crescent);
- buildings at intersection of Boronia and Dorset Roads (217-229 Dorset Road and 114-132 Boronia Road);
- service station (158 Dorset Road);
- maternal and child health centre fronting Boronia Road (136 Boronia Road); and
- the Boronia Progress Hall (134 Boronia Road).



Early built form fabric from the 1920s, 1960s and 1970s represent Boronia's significant periods of growth and development.

Colours & Materials of Boronia

A series of colours & materials schedules accompany the Precinct Plans contained within Section 6 of this report. These schedules are to be used to guide future buildings and works within the Activity Centre, to ensure *"that the colour and form of buildings reinforce the emerging contemporary natural character, including the naturalistic theme of the local area and the identified precinct areas."*

The colour & materials schedules take into consideration the existing landscape, the current use of colour in Boronia, and the proposed visions for the future growth and development as outlined in the Boronia Structure Plan.

While some existing building colours in Boronia gel with the natural surrounds (such as the natural brickwork and deep renders) many building colours of Boronia such as the blue, teal and sand, are more reminiscent of a seaside town than that of a bush based village. The colour & material pallets move away from any pastels and encouraging a deeper toned neutral coloured environment, with small inserts of intense earth tones.

When considering colour, consideration should be made for the size of the area it is applied to. This will include deep background tones for large areas (to blend with the hillside) and bright colours for smaller shopfronts and accessories. In addition to colour, materials with texture must be used to help bridge the gap between the natural landscape and the commercial environment.

Many of the building colours used within Boronia, including blue teal and sand are reminiscent of a seaside town than of a bush village.

The suggested colours aim to provide enough flexibility for different businesses, while inspiring participation in the future vision for Boronia.

The deeper toned neutral colour and materials suggested will build on the natural character and hillside setting of the local area.



5.4 Accessibility Framework

(Refer to Figure 8 & 8(a))

Within the study area, the Accessibility Framework Plan outlines a desired transport and traffic structure, to complement the directions of the Pedestrian Space and Land Use Framework Plans. The Accessibility Framework supports the development of more sustainable travel behaviour (including public transport and pedestrian and bicycle movement) and improved traffic and parking conditions in Boronia. Key issues to be addressed in the Accessibility Framework Plan include improvements to pedestrian priority and access between all areas of interest, extensions to the bicycle network, and improving directionality and signage for vehicles.

Public Transport

The ongoing improvement of the Boronia railway station and interchange area will continue to provide a manned focal point for public transport services within the Boronia Activity Centre. Development of the existing car park sites around the station area will provide opportunities to accommodate a mix of uses, whilst ensuring there will be no net loss of commuter car parking in walking distance of Boronia railway station. This will increase safety within this precinct, by increasing natural surveillance over the transit area. Commuter parking may include the use of the Boronia Park car park as an overflow commuter parking area during the week as it is within close / convenient walking distance of the station area. Encouraging the use of the Boronia Park car park for overflow commuter parking should be met with traffic management works to ensure minimal through traffic along Park Crescent thereby minimising the disturbance to local residents.

Council and the community will continue to lobby the State Government for a sustained commitment to upgrade the Principal Public Transport Network.

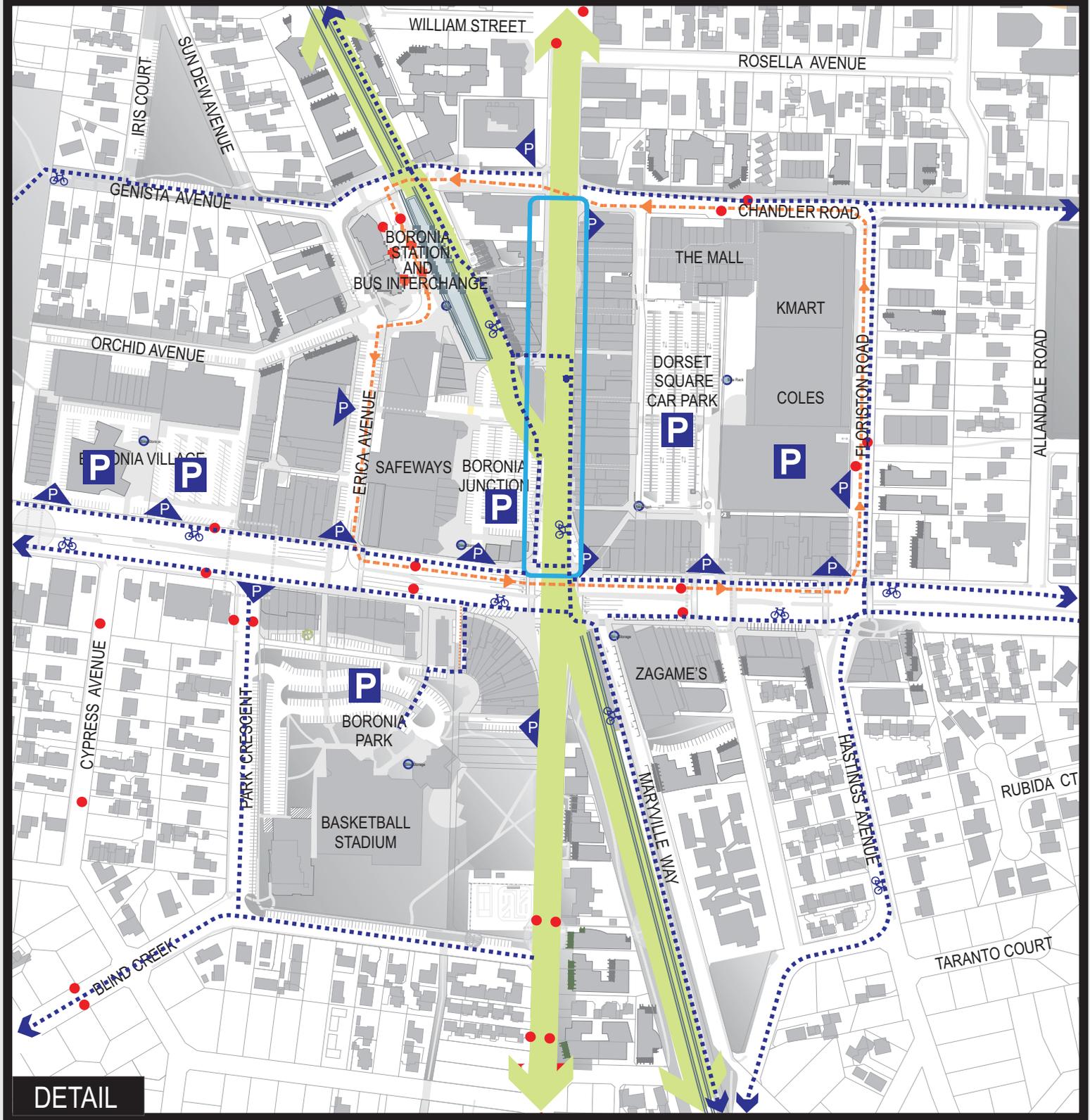
Improvements to the local public transport service will include:

- increased spread of hours, frequency and interconnectedness between services;
- advocating for more affordable pricing of public transport. Currently Boronia is located within Zone 3 making it cost prohibitive to travel across zones, particularly to access the Melbourne CBD;
- the provision of bus stops and services that enable safe and convenient access for users of all ages and abilities;
- discouraging the use of the bus interchange by private vehicles, by introducing a designated drop-off and pick-up zone and taxi rank within Lupton Way and on Erica Avenue;
- improving the level of amenity and urban design quality within the station and interchange environments;



- STUDY AREA
- OFF-STREET CAR PARK AREA
- DIRECTIONAL SIGNAGE TO CAR PARK AREAS
- PRINCIPAL PUBLIC TRANSPORT ROUTE (PPTN) (MELBOURNE 2030)
- RAILWAY LINE / RESERVATION
- INVESTIGATE PROVISION OF TRAFFIC LIGHTS / ROUNDABOUT.
- ANTI-CLOCKWISE BUS LOOP
- BUS STOPS
- INVESTIGATE THE PROVISION OF 12-SEATER BUSES, AT 20M INTERVALS, PROVIDING MORE FREQUENT LEVEL OF SERVICE IN DORMITORY AREAS, LATE AT NIGHT AND ON WEEKENDS
- DORSET ROAD: REDUCE TRAFFIC SPEED AND IMPROVE PEDESTRIAN CONNECTIONS
- BIKE PATH LINKS

Figure 8
boronia *Your Place, Your Life, Our Future*
 Structure Plan: Accessibility Framework
 (October 06)



DETAIL

- OFF STREET CAR PARK AREA
- DIRECTIONAL SIGNAGE TO CAR PARK AREAS
- PRINCIPAL PUBLIC TRANSPORT ROUTE (PPTN) (MELBOURNE 2030)
- RAILWAY LINE / RESERVATION
- INVESTIGATE PROVISION OF TRAFFIC LIGHTS / ROUNDABOUT
- BUS STOP
- DORSET ROAD: REDUCE TRAFFIC SPEED AND IMPROVE PEDESTRIAN CONNECTIONS
- BIKE PATH LINKS
- BIKE STORAGE / RACK
- ANTI CLOCK WISE BUS LOOP / BUS STOP

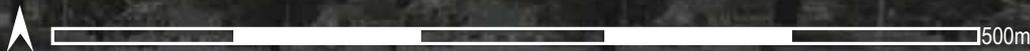


Figure 8(a)

boronia *Your Place, Your Life, Our Future*

Structure Plan: Accessibility Framework Plan

(October 06)



- the provision of an alternative means of accessing the commercial environs, and ensuring access for the elderly and non-car users. Investigate the provision of a community bus offering frequent levels of service in dormitory areas, late at night and on weekends;
- the provision of an anti-clockwise bus loop and series of stops to provide direct access to all precincts within Boronia. The loop commencing at the interchange will traverse:
 - Erica Avenue
 - Boronia Road
 - Floriston Road; and
 - Chandler Road
- the provision of safe, attractive and well-lit all-weather bus stops at identified locations;
- advocating for the provision of a bus route connection to Knox Leisureworks;
- continued advocacy for increased provision of public transport off peak during the evening, middle of the day and on weekends;
- advocacy for the establishment Smart Bus facilities to Boronia;
- advocating public transport provides best practice access for all, including the implementation of a low rider bus system improving access for persons with limited mobility; and
- the provision of maps and directional signage at bus stops and the station.



Defined on- and off-road bike paths will provide links between the commercial area, schools, recreational and community facilities. The provision of appropriate end of trip storage facilities should be provided in convenient and safe locations.

Cyclists

A series of defined on- and off-road bike paths will provide links from the regional Principal Bicycle Network (PBN) along the railway line, Boronia and Dorset Roads, to all precincts and recreational facilities within the Boronia Activity Centre. The bike paths will have high quality surface treatment and those in high volume traffic areas will be defined in contrasting colour. Where there are changes in direction clearly identifiable signage will be erected.

Convenient bike parking will be provided throughout the centre, including the increased provision of bike locker facilities and bike racks at the Station. A minimum of twenty undercover parking rails will be provided in the commercial environs, with five rails at each community facility including the Library, Knox Leisureworks, the Basketball Stadium and within reserves.

New developments within the commercial environs will offer bike storage and shower facilities for staff, while more substantial residential development will incorporate bike storage facilities towards the front of sites to encourage the use of bikes as an alternative means of transport to the centre.

Further issues and improvements include:

-
- re-allocating Boronia Road road-space through the centre to provide an on-road connection to the proposed Forest Road bike path;
 - extending the Albert Avenue on-road path through to Boronia Road as a dedicated cycle lane;
 - reviewing the north-south bicycle connection along Dorset Road and exploring the removal of the right hand turn lane into Boronia Junction to provide sufficient road width to incorporate a designated on-road bike lane. Alternate principal access from Erica Avenue and Boronia Road will be signed.



Traffic and Parking

Vehicular management within Boronia will be a critical challenge given Boronia's location around the primary arterial cross intersection of Dorset and Boronia Roads. Improved management of traffic within Boronia will include:

- seeking opportunity to improve vehicular flows through the centre with the provision of linked traffic signals. This principle should be applied where it is proposed to provide additional pedestrian lights or signals within the commercial environs;
- reducing traffic speeds and improving pedestrian connectivity and amenity along Dorset Road. This will be achieved through:
 - provision of a raised centre landscaped median between Chandler and Boronia Roads, to reduce through traffic speeds;
 - a widened pedestrian crossing and shared vehicle pedestrian zone (with minimal variations in grade) between the station and Dorset Road; and
 - Seeking Victoria Police enforce speed limits within Boronia, with a particular focus on identified problem areas, including the 40km zone along Dorset Road, Erica Avenue, Chandler Road, Floriston Road & Herbert Street.
- investigating the provision of additional traffic lights at the intersection of Turner Road, Torrens Avenue and Boronia Road (point of entry/exit into Dorset Square) to improve vehicular ingress and egress and increase pedestrian connectivity north-south across Boronia Road;
- investigating the provision of traffic lights to facilitate east bound right turning traffic from Park Crescent into Boronia Road;
- monitoring the traffic activity, safety and pedestrian demands at the Boronia Road/ Tormore Road/ Narcissus Avenue intersection to justify any future signalisation;
- providing prominent directional car parking signage throughout the centre to alert drivers to preferred car parking locations and access routes;
- installing traffic calming measures along the *ring road*, particularly proximate to defined pedestrian crossings;
- undertake an assessment of the through traffic conditions along Sundew Avenue and Herbert Street between Scoresby Road and the Boronia commercial area to determine the need for traffic calming, or the potential closure of this through route;
- Undertake an assessment of the need for traffic calming, along Elsie Street; and
- relocating the 40 km/h sign across Dorset Road south of Elsie Street, to north of the existing school crossing facility.

The 'ring road' provides access around the northern edge of the retail area within Boronia. The ring comprises Erica Avenue, Chandler Road & Floriston Road from Boronia Road west to Boronia Road east

A rational approach to the provision of parking facilities will be adopted in Boronia. This will help improve access to vibrant commercial precincts and facilities within the study area. In particular:

- a series of directional signs for the centre will be added. These signs will direct drivers from the main road approaches to major off-street car parking locations including:
 - Chandler Road access to the Dorset Square uncovered car park;
 - The Floriston Road point to the Dorset Square deck car park;
 - Erica Avenue at Boronia Road (to the railway car park);
 - Boronia Road into the Boronia Park car park (particularly to direct overflow commuter parking);
 - Boronia Road into the Boronia Village car park.
- the efficiency, amenity and operation of car parks will be maximised by:
 - providing wider spaces and aisles slightly in excess of the Australian Standards;
 - simplifying movement and accessibility;
 - simplifying signage and line marking;
 - providing frequent & convenient locations for trolley bays throughout the extent of off-street car parks;
 - Provide parents with pram parking spaces in convenient locations.
 - undertaking substantial landscape treatment and canopy tree planting to improve amenity;
 - introducing dedicated pedestrian right of way paths ;
 - where redevelopment is proposed seek the incorporation of sufficient and visually stimulating lighting, resurfacing and provision of a lift/ for access to the deck car park in Dorset Square. The regrading of vehicle ramps to improve access may be considered in any redevelopment of the deck;
 - removing obstructions that clutter and congest and which confuse drivers in car park areas, namely:
 - the removal and relocation of the compactor facility and toilet in Dorset Square;
 - relocation of the petrol station from the Junction car park to an approach into the centre.
- providing direct pedestrian connection between Erica Avenue and Boronia Park which will allow the Boronia Park car park to be used for overflow commuter parking during the week, and will provide opportunities for the development of VicTrack car parking sites adjoining the station and interchange environs;

-
- providing short term parking (mainly 2P with short term spaces in locations adjacent to the edge of the car park) in commercial off-street car park areas;
 - Providing select areas of 4P during the day & unrestricted night time parking in vicinity of the Metro Cinema complex & Dorset Square south night time precinct;
 - providing disabled parking in locations to a minimum of 2% of overall supply. They will be on level grade areas and adjacent to walkways and building entry points;
 - locating Taxi ranks adjacent to pedestrian pathways and retail attractors including the station environs and core commercial attractors;
 - designating loading zones for commercial operators; and
 - designated areas for bus drop off and pick up (e.g. for community bus & retirement village bus use).

5.6 Social Well Being

It is critical to develop a stronger and more vibrant Boronia, with positive improvements to social image, public safety and perceptions of the retail precinct.

Crucial in defining community pride and an active community life, is the need to build connections within the community, and to identify and support talented leaders. This will be achieved by supporting community groups, strengthening the trading community, fostering a youth culture and respecting the needs of our ageing population. A strategic approach to engage, involve and encourage commitment, responsibility and ownership will lead to increased community pride and promotion within the Boronia community.

Objectives of the Social Well Being Framework include:

- creating opportunities for active participation in community life;
- recapturing and regenerating assets that have been lost or forgotten in the Boronia community. This may also involve removing *unwanted assets*;
- building community connections, including those between young people and the broader community;
- creating a physical environment that the whole community can be proud of;
- supporting and developing talented leaders within the community;
- understanding, honouring, valuing and fostering, the cultural heritage of Boronia;
- providing equal opportunities for active participation in community life;
- achieving physical, social, cultural, environmental and commercial success in Boronia.

Removing "unwanted assets" in accordance with Council policy will go through an open and transparent process & will require thorough community comment & response.

Criteria & formal guidelines should be determined that outlines an objective triple bottom line assessment.

The criterion to identify an "unwanted asset" must demonstrate the disposal or exchange will provide an outcome which provides greatest benefit to the Boronia community.

The opportunities and requirements for a stronger community (mentioned in the Framework Plans) include:

- the provision of quality public environments including a community hub or precinct, and public spaces for informal gathering, that support local organisations, festivals & events;
- the betterment of Boronia as a vibrant local retail destination;
- the provision of quality residential environments;
- greatly improving safety in Boronia, particularly around the station and laneways. A particular focus must be provided for areas where we are seeking more vibrant public spaces or that are to be used in the evenings, which could threaten people's feelings of safety within public spaces;

- Develop partnerships to improve safety through the Knox Safer Communities Committee, Boronia Police and the community;
- providing quality public facilities and infrastructure.

Actions

- Establish a community group for Boronia to be a collective voice, in driving change and building the community.
- Ensure new use and development does not undermine or conflict with the above stated objectives and opportunities.
- Establish a relationship between Boronia Police, the community & Council to improve safety within the commercial environs. This group will explore opportunities to improve perceptions of safety within Boronia and may include:-
 - increased visual police presence in the Boronia Activity Centre;
 - a focus on safety at Boronia Station, including a review physical elements which may include (although are not limited to) improved lighting, establishing security cameras and providing music, alongside a holistic Crime Prevention Through Environmental Design review;
 - establishing a volunteer foot patrol within the centre; and/ or
 - other associated techniques.
- Require future development and works to address 'crime preventative' environmental design principles.
- Undertake a detailed business and community development program encompassing positioning of the commercial environs and individual precincts, a tenancing strategy and select micro-business improvement programs, to contribute to the development of an attractive Activity Centre.
- Build on existing examples to increase public art in Boronia. This may include the use of art installations, lighting, vacant shop fit outs, historic murals, displays from local artists and youth and community art projects.
- The establishment of a program of community events. These may be community run and business supported.
- Conduct community planting days including the re-establishment of a rose garden, seasonal feature planting in the commercial environs and landscaping in Boronia Park.
- Providing opportunities for the identification and interpretation of heritage in the local area.

-
- Seek opportunities for commercially supported community breakfasts.
 - Provide continued support for the Boronia Basin Community News.
 - Establish community noticeboards within Boronia, providing a regular space for community updates, scheduled programs, or updates on the Boronia Place Management Program.
 - Seek and deliver testimonials from the local community which talk up the positive aspects and celebrate the achievements of individuals in the local community.
 - Seek programs which provide music for all age groups in the centre. This may include a free music program in the Village or square.
 - Reconnect isolated people by utilising their talents and encouraging them to make their own decisions.
 - Utilise existing community spaces for community services within commercial precincts and recreational centres.
 - Document and communicate the social history of Boronia.
 - Establish a feedback system to create a cycle of communication connecting all levels of the community and governing bodies.
 - Establish a commitment to cleanliness and maintenance focusing on the commercial environs.

5.7 Art in Public Places

Knox has a long history of developing art in public places and is currently developing a public art policy as part its commitment to the arts and culture. This Art in Public Places Framework for Boronia links to the draft Art in Public Places Policy.

Art in public places, whether permanent or temporary, exists to provide an interpretive or contrasting creative element to a space or place. Public art, both commissioned works and community projects, can build on the sense of place, identity and assist in building the social dimension of Boronia.

Public Art in Boronia

Within Boronia it is preferred if work is referential of the site or site specific, however it is an art work and can exist outside of these parameters.

It is represented by a great diversity of media that is often mixed and integrated within architectural, urban and landscape design. It includes but is not restricted to metal and wood craft, casting of materials, sculpture, mural painting, relief works, photography, light projection, film, video, digital works, sound works, ceramics, earth works, environmental works, drawing, printmaking, painting, performance, light and text.

The scale of works is not fixed. Art in public places may be grand, monumental or formal. Alternatively they may be small, subtle, fragmented and fleeting.

Why Create New and Contemporary Work in Boronia?

It is often perceived as difficult for local communities to be involved in the creation of new and contemporary artworks. New work is challenging to the viewer or to the community. It is sometimes difficult to understand its concept or meaning. This can be overcome with interpretive information and explanation, but more often the passage of time builds a strong connection to the artwork.

Boronia and Art in Public Places

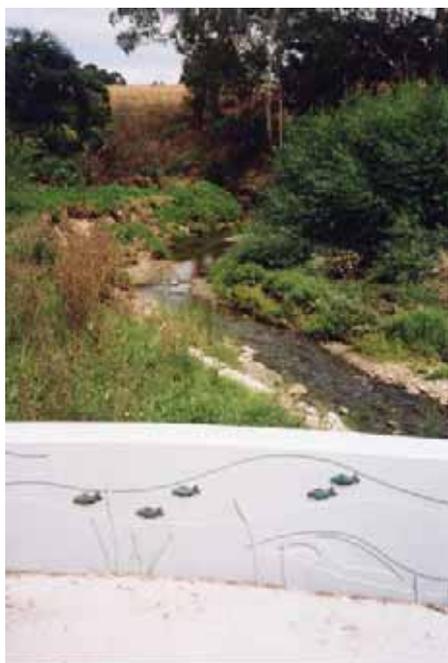
The Boronia Structure Plan takes a holistic approach to design that strategically integrates many elements to achieve a better place and a stronger more dynamic community. Consequently art in public places is integrated within this structure plan.

It is also essential to physically integrate works as part of the environment, whether natural or built, and to provide work that is progressive, creative, contemporary, challenging and stimulating to the community.

In Boronia, integration will occur by engaging art professionals at the earliest possible time in the development of public projects. Artists will work collaboratively with architects, planners, landscape architects and other design professionals on projects.



*Journey Seed
(Box Hill)
Artist: Anthony McInnery*



*Placemakers
(Boronia Road underpass)
Cast concrete wall with bronze fish
Artist: Lu Skacej 1997*



*The Books
(Springvale)
Artist: Geoff Hogg*

Community Partnerships

Engagement and consultation with the community is an area with much room for expansion. Usually, the community is defined as a mass entity rather than a series of communities that may be small organisations, friends groups, neighbourhoods, users etc. Other communities need to be considered for the purposes of art in public places and arts programs generally.

In Boronia, business is a possible source of funding. Their workforces are also communities and as such may be the active participants in a project. Schools, TAFEs and Universities and their staff and families are all smaller communities within the whole.

For Boronia there will be strong community connection in the development of every art project. There are many ways to have community involvement in projects.

People may:

- inspire the project;
- participate in decision making through a public art reference group;
- participate in concept and design;
- participate in construction;
- help with installation;
- promote the project;
- work on project committees;
- provide important local advice;
- develop project themes;
- help record the project;
- own local businesses that service the project.



*Colliding Worlds.
Youth Art Project
Knox City Council*

Funding options

Both Percent for art and the Planning incentive funding policies below are currently being developed.

Percent for Art

Percent for art is a funding policy that dovetails with the draft Public Art Policy and provides separate guidelines for the sustainable funding of projects across the municipality.

Guidelines for the policy include a sum of 1.5% to be allocated to art in public places from the construction costs of all Council capital improvement projects above one million dollars and 10% of the 1.5% to be reserved for the ongoing maintenance cost of public art.

In landscape works 5% of the allocated budget would go to developing public art. Percent for art is essential for all major development works in Activity Centres.

Planning Incentive Policy

The planning incentive policy is similar to the percent for art policy. It applies to all new developments.

The threshold for implementation of the scheme requires assessment, however it may be 1.5% where a development is over a certain amount or be applied based on floor space, ie \$50,000 for every 10,000 metres of floor space.

Planning Incentive is essential for all major development works in Activity Centres. Council works in partnership with developers to ensure that projects are managed for the best possible community outcome.



*Bench
(Springvale)
Artist: Penny Algar*

Government Funding

Each of these funding methods may be complimented by partnerships with other spheres of government. Possible sources of funding include Arts Victoria, Vichealth, Department of Infrastructure and Planning, Department for Victorian communities and the Australia Council for the Arts.

Site identification in Boronia

Many sites have been identified within the precinct plans, however more detailed work will be done to develop a series of sites and approaches. Rather than simply allocate spaces for sculpture, artworks will work seamlessly with the natural and built environment. There is a limit to the funding available so to ensure maximum impact of works siting will be considered strategically.

Sites with a priority are those that are:

- highly visible,
- very accessible,
- are used by different groups,
- a catalyst for future activity,
- provide innovative and creative opportunities

Linkages with other frameworks in this structure plan

The art in public places framework dovetails with other frameworks included in the Boronia Structure Plan. It will provide the guidelines for the 'incorporation of artwork along blank walls or car park areas' as stated in the built environment framework. It provides options for the development of a 'stronger and more vibrant Boronia' and a method of community engagement to engender social well being in the accessibility ramework.

Its greatest impact on these frameworks will be through the Pedestrian Space & Precincts Plan, the central component of the structure plan.



*Newport Railway Station
Artist: Geoff Hogg*

Pedestrian Space and Precincts: The role of artworks

There are 6 precinct plans within the Boronia Structure Plan, each with a catalytic project. The linkage between precincts is brought about by a 'back of road', or 'quiet street' connection identified as the Principal Pedestrian Link. The principle pedestrian link will be used as the map for identifying sites for the successful integration of art works throughout Boronia. Artworks may be developed and used along the entire link perhaps in repetition or as a repetitive concept that engages at a human or pedestrian level.

These links present intimate spaces and places where art works provide tactile interaction. The pedestrian experience is enhanced by stopping, considering, touching and thinking about the art work integrated within the pedestrian link.

Alternatively, roadside works are often experienced fleetingly and at speed. At intersecting points with major roads, larger works should be planned to provide a more substantial focus. They provide the engagement to draw the passing motorist more deeply into pedestrian areas.

Therefore Boronia junction, the Dorset Rd Precinct Plan and the link to Cinema Lane offer an exciting opportunity for art works that are highly visible, accessible and entice people to explore other parts of Boronia.

Actions

- Development of a detailed scope and feasibility for the Cinema Lane project
- Development of a detailed framework for art in public places
- Identify potential financial partners to develop projects
- Identify community groups, business groups and interested parties.
- Complete site analysis and investigate funding opportunities for individual projects.

6.0 Precinct Plans

The most significant challenge within the Boronia Activity Centre is the barriers created by roads and rail corridors. The road and rail (despite the undergrounding of the railway line) infrastructure has resulted in a disconnected centre, with five separate complexes. The Framework plans in Section five propose a series of strategies to aid linkages between areas of interest and to define a series of identifiable precincts.

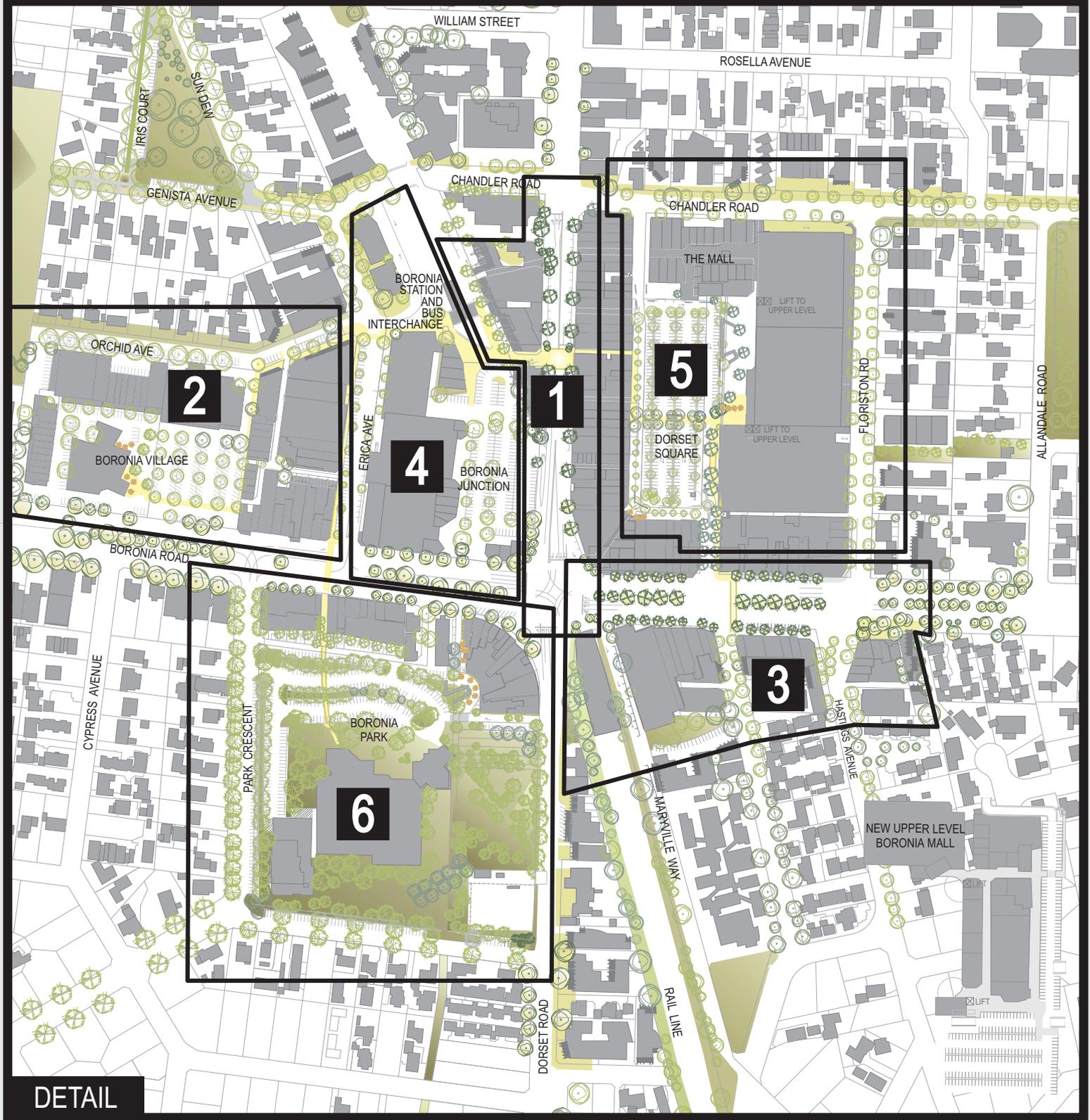
The Structure Plan Framework comprises six precincts (figure 9):

1. Dorset Road
2. Boronia Village
3. Southern Terraces
4. Boronia Junction
5. Dorset Square
6. Boronia Park

The precincts are clustered and connected by the Principal Pedestrian Linkage. Collectively, the projects within the precincts (in both the public and private realms) will facilitate the achievement of the Strategic Directions and broader Vision for Boronia.

The Precinct Plans will require a range of interventions, project facilitation, public works and private developments. The following section:

- describes each area and the context and opportunities of that site and its surrounds;
- presents a statement of how the precinct will look in the future;
- proposes actions and project elements required to achieve 'our future'.



DETAIL

- 1** DORSET ROAD
- 2** BORONIA VILLAGE
- 3** SOUTHERN TERRACES
- 4** BORONIA JUNCTION
- 5** DORSET SQUARE
- 6** BORONIA PARK

- OPEN SPACE
- PRINCIPAL PEDESTRIAN LINKAGE
- BUILDING FOOTPRINT



1

6.1 Dorset Road

(Refer to Figure 10)

Context

Dorset Road represents a retail strip between Chandler and Boronia Roads. Historically a series of retail arcades provided access between the Station and Boronia Mall, but the relocation of the pedestrian lights redirected pedestrian movement, resulted in a series of underutilised arcades. This has led to little reinvestment into buildings and shopfronts in recent years and consequently the arcades contain inactive tenancies, have low lighting and provide a low level of amenity and perceived safety.

Perceptions of Dorset Road are further challenged by:

- a dominance of vehicular traffic, including traffic noise and speed;
- the lack of direct and safe east-west crossing points between Dorset Square, the station and Boronia Junction;
- poor quality paving and streetscape furniture, lack of trees, and excessive highway scale lighting along the street;
- discontinuous building heights and form and excessive levels of signage and visual clutter;
- a lack of visual clues directing through traffic to adjacent retail and commercial areas and in particular, to retail attractors in the precinct.

Our Future

"Dorset Road is a highly amenable through traffic route. A series of east-west pedestrian crossing points provide convenient and direct pedestrian access between areas of interest including a southern crossing point which provides direct evening access between the cinema and the bowling centre. A wide pedestrian crossing between the station (via the shared pedestrian vehicle zone on Lupton Way) and Dorset Square, provides access at all times of the day and night.

Substantial improvements in the streetscape have built on the amenity and outlook of Dorset Road towards the Lysterfield Hills. Works have included a raised landscaped central median, and contiguous street tree planting which provides shade and creates uniformity along the street. A feature lighting scheme, improved paving, new seating and directional signage, have created a comfortable pedestrian retail strip along this busy road.

Lupton Way is principally a pedestrian environment although limited vehicular access is provided for loading, and to access existing car park areas. The redeveloped shops and arcades to the east of Lupton Way now face the station. The upper level apartments and offices provide housing within the centre, and overlook Dorset Road and the station from balconies and openable windows. The apartments make this area a safe place to visit at all times of the day and night."



View south along Dorset Road from Chandler Road
(existing conditions within inset photo)



- 1 Mixed use residential-commercial development on Victrack and vacant land north of Chandler Road. Provide underground commuter parking. Ensure development provides an address adjoining pedestrian pathways and overlooks the station environs.
- 2 "Green Link" pedestrian and bike path between Tomore Reserve and Chandler Park.
- 3 Provide landscape treatment to visible northern edge of Boronia Police Station. Incorporate public art/architectural detailing to fence to. Planting should reinforce pedestrian scale and provide enclosure along the ring road system.
- 4 Provide continuously accessible paths of travel for pedestrians through the Chandler Road/Dorset Road intersection. Ensure the grade of the pedestrian path provides access for all. Improve length of times on lights to facilitate Pedestrian movement. Undertake works to slow vehicle speeds southbound along Dorset Road.
- 5 Promote the use of a pick-up/drop off point in Lupton Way as an alternative to use of the bus interchange (by private vehicles). No right hand turn into Dorset Road. Re-route to provide south bound one way traffic movement from Chandler Road to Dorset Road. Facilitate shared vehicle-pedestrian zone through textured road surface to Slow vehicles.
- 6 Directional signage for vehicle access to off-street car park areas via the northern ring road system.
- 7 Construct median landscape inset along Dorset Road and canopy street trees with feature uplighting.
- 8 Refurbish/redevelop arcades between Dorset Road and Dorset Square with adequate lighting and active frontages. This may require site consolidation to ensure minimum store dimensions to accommodate sustainable retail and active retail service tenancies.
- 9 Improve permeability onto station platform at grade pedestrian pathways. Ensure sufficient levels of lighting around the interchange and pathways to the station.
- 10 Intensive development along Dorset Road: incorporate pedestrian overpass/sky-walk to connect the station to Boronia Mall.
- 11 Upper level development to encroach over Lupton Way to provide passive surveillance opportunities over station area. Development to incorporate active frontages towards the interchange to address security issues.
- 12 Advocate manning of Boronia Station. Provide signage to adjoining areas of interest, seating rest areas for commuters and public art.
- 13 Investigate future crossing point.
- 14 Remove or design access barriers within arcades to improve visibility for the vision impaired and improve access for wheel chairs.
- 15 Shared pedestrian-vehicle zone : Provide minimal variance in grade changes along the 'Principal Pedestrian Path' from Dorset Square to Boronia Station point of entry. This will further improve visibility from the station platform from Lupton Way.
- 16 Ensure active edge to the 'Principal Pedestrian Path'. Provide 24-hour access between the station and Dorset Square car park.
- 17 Construct widened footpath/contrasting pavement at central Dorset Road crossing point. Lengthen crossing time of pedestrian lights. Ensure sufficient footpath widths (preferably 5 metre minimum) for access along Dorset Road, particularly adjacent to the new retail development where the footpath is narrow. Remove/relocate street poles/lights that obstruct the direct path of travel. Provide pedestrian priority in shared pedestrian-vehicle zone along Lupton Way. Limit future development vehicle access via Lupton Way from Dorset Road.
- 18 Seek continuance of pedestrian path. Will require acquisition of existing car parks under private ownership. Investigate the use of porous terrafirma or like porous paving material at base of existing trees. Seek long term maintenance and replacement of existing eucalyptus to maintain substantial canopy spread and screening whilst providing a path. Incorporate public art to screen rear of commercial development.
- 19 Remove unnecessary crossovers along Dorset Road.
- 20 Undertake detailed design for the removal of the right hand turning lane from the northern approach to minimise congestion and provide sufficient width for an on-road bike path connecting to the rail train. Left turn in, left turn out only.
- 21 Provide public art and display space to showcase local artists and youth art.
- 22 Reduce lane/slip lane widths and widen pedestrian path.
- 23 Provide continuously accessible paths of travel for pedestrians across the Boronia/Dorset Road Intersection. Improve length of times of lights to facilitate pedestrian movement, particularly for an ageing population. Minimise distance from kerb to kerb and increase footpath areas. Undertake traffic calming on slip lanes. Improve definition of pedestrian crossing areas. Remove/relocate signage poles located in direct pedestrian path of travel.
- 24 Remove slip lane to facilitate pedestrian/bike access across the intersection.
- 25 Improve directional signage on bike tracks. Improve the definition and provision of paths through the centre and between areas of interest.
- 26 Directional sign to police car park.

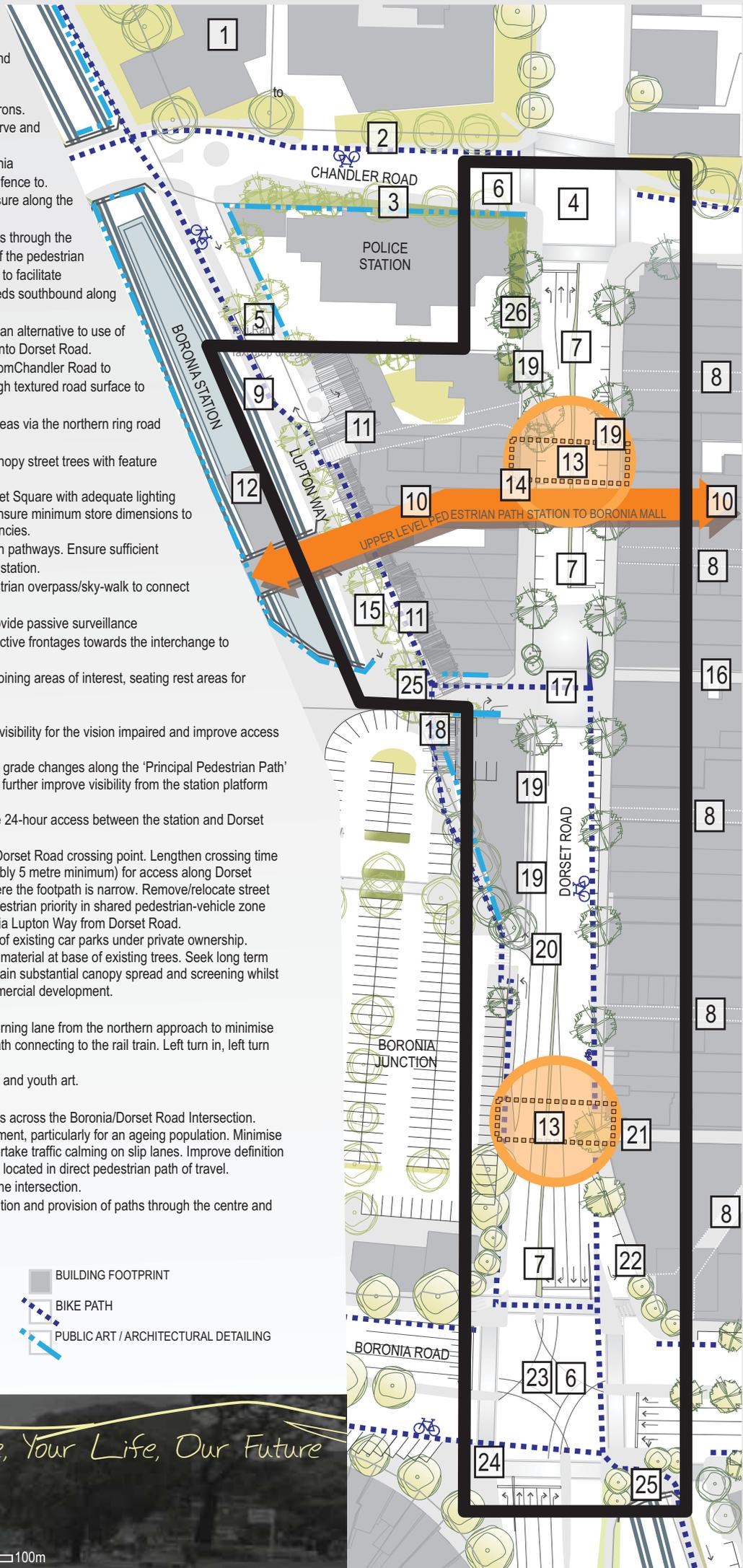
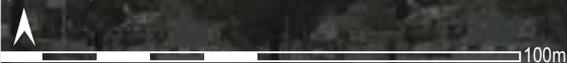
- | | |
|---|---|
|  INVESTIGATE FUTURE PEDESTRIAN CROSSING |  BUILDING FOOTPRINT |
|  DORSET ROAD PRECINCT AREA |  BIKE PATH |
|  OPEN SPACE |  PUBLIC ART / ARCHITECTURAL DETAILING |
|  PEDESTRIAN SPACE SYSTEM | |

Figure 10

boronia *Your Place, Your Life, Our Future*

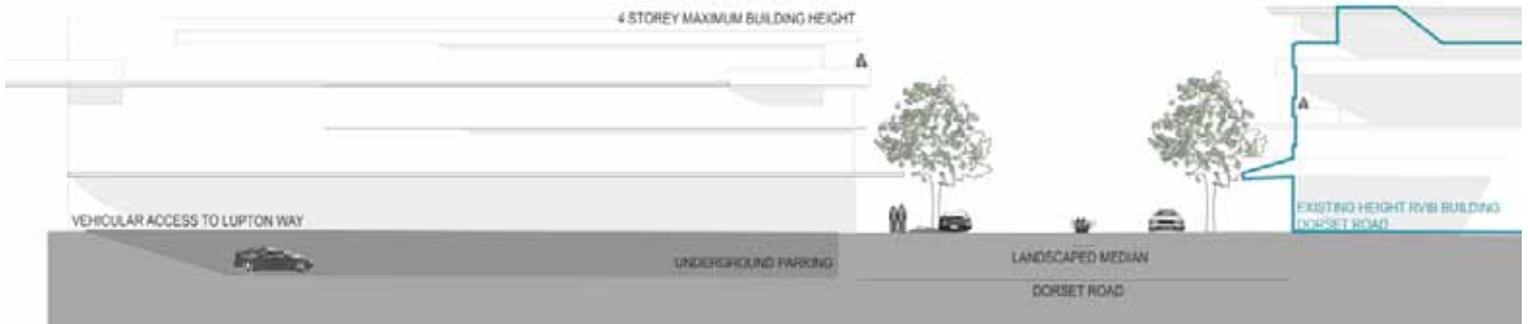
Precinct Plan 1: Dorset Road

(October 06)



Actions and Project Elements

- Facilitate east-west movement between the station area and Dorset Square, through the construction of a wide pedestrian crossing across Dorset Road, and a shared pedestrian vehicle zone along Lupton Way.
- Ensure sufficient footpath widths (5 metre minimum) in all new developments along Dorset Road. Remove and relocate street poles and lights that obstruct direct path of travel. Incorporate new paving and canopy street trees to provide vertical structure and shade and to screen development. Provide feature up lighting of trees.
- Construct a raised central median strip along Dorset Road to slow traffic and facilitate safe pedestrian crossing at defined points. Provide a continuously accessible path of travel for pedestrians through the Chandler Avenue/Dorset Road intersection. Increase the duration of pedestrian lights.



Indicative section (Dorset Road to Lupton Way) illustrating building heights, street planting & landscaped median.

- Review interest in the development of a mixed-use residential–commercial development on the VicTrack site and vacant land north of Chandler Road. Provide undercroft commuter parking, ensure an address is provided to the adjoining pedestrian pathways, and ensure openable windows and balconies overlook the station area.
- Landscape the prominent northern edge of the Boronia Police Station. Incorporate public art or detailing on the fence for visual interest. Planting in this location should reinforce pedestrian scale and provide enclosure on the pathway adjacent to the ring road system.
- Promote the use of a pick-up/drop-off point by private vehicles in Lupton Way, as an alternative to illegal use of the Bus Interchange. Provide one way movement from Chandler Avenue to Dorset Road and allow limited vehicle access to identified premises only. The shared vehicle pedestrian zone should incorporate textured road surface to slow vehicle movement. The pedestrian space along Lupton Way will be of sufficient grade to allow direct lines of sight onto the station platform. The variances of grade that are a challenge for pedestrians, particularly those of limited mobility, will be removed.

- Refurbish and redevelop arcades between Dorset Road and Dorset Square with adequate lighting and active frontages. This may require site consolidation to ensure minimum store dimensions accommodate sustainable retail and active retail service tenancies.
- Where intensive development along Dorset Road is imminent, seek the construction of a pedestrian overpass or skywalk to connect the station environs to Boronia Mall. Any proposed overpass or investigations to build infrastructure over the rail line must be assessed by VicTrack and Department of Infrastructure. A joint partnership project will be sought by Council in conjunction with the State Government and the private sector.
- Upper level development near the station could extend over and overlook Lupton Way to increase opportunities for passive surveillance over the station area. Development around the station must incorporate active frontages towards the interchange to address security issues.
- Clearly define pedestrian links and connectivity around the station area and provide signage to adjoining areas of interest. Provide seating and rest areas for commuters.
- Seek continuance of the regional bike path through the commercial areas. This will require acquisition of existing car parks under private ownership in order to construct a dedicated path and adjacent landscaping. Investigate the use of porous paving material at the base of existing trees along this pathway, and seek long-term maintenance and replacement of existing eucalyptus to maintain a substantial canopy spread and screening. Incorporate public art to screen the rear of the commercial development bounded by Dorset Road, Lupton Way and the Junction.
- Undertake detailed design for the removal of the right hand turning lane (into Boronia Junction) from the northern Dorset Road approach to minimise congestion and provide sufficient width for a designated on-road bike path to connect the rail trail. Left turn in and left turn out only, should be allowed. This will require directional signage for vehicle access to the Boronia Junction off-street car park area, via the northern ring road system and Boronia Road.
- Directional signage through the centre and between areas of interest on the bike tracks is critical through the commercial area.
- Review the intersection of Boronia and Dorset Roads to increase pedestrian and bicycle safety and amenity. Investigate the potential to reduce and/or remove slip lanes, widen pedestrian paths and minimise the distance from kerb to kerb. Ensure continuously accessible paths of travel for pedestrians across the Boronia/Dorset Road intersection and highlight pedestrian crossing areas. Increase the duration of crossing times at lights to assist pedestrian movement, particularly for an ageing population. Where appropriate at this intersection remove or relocate signage poles located in direct paths of travel.

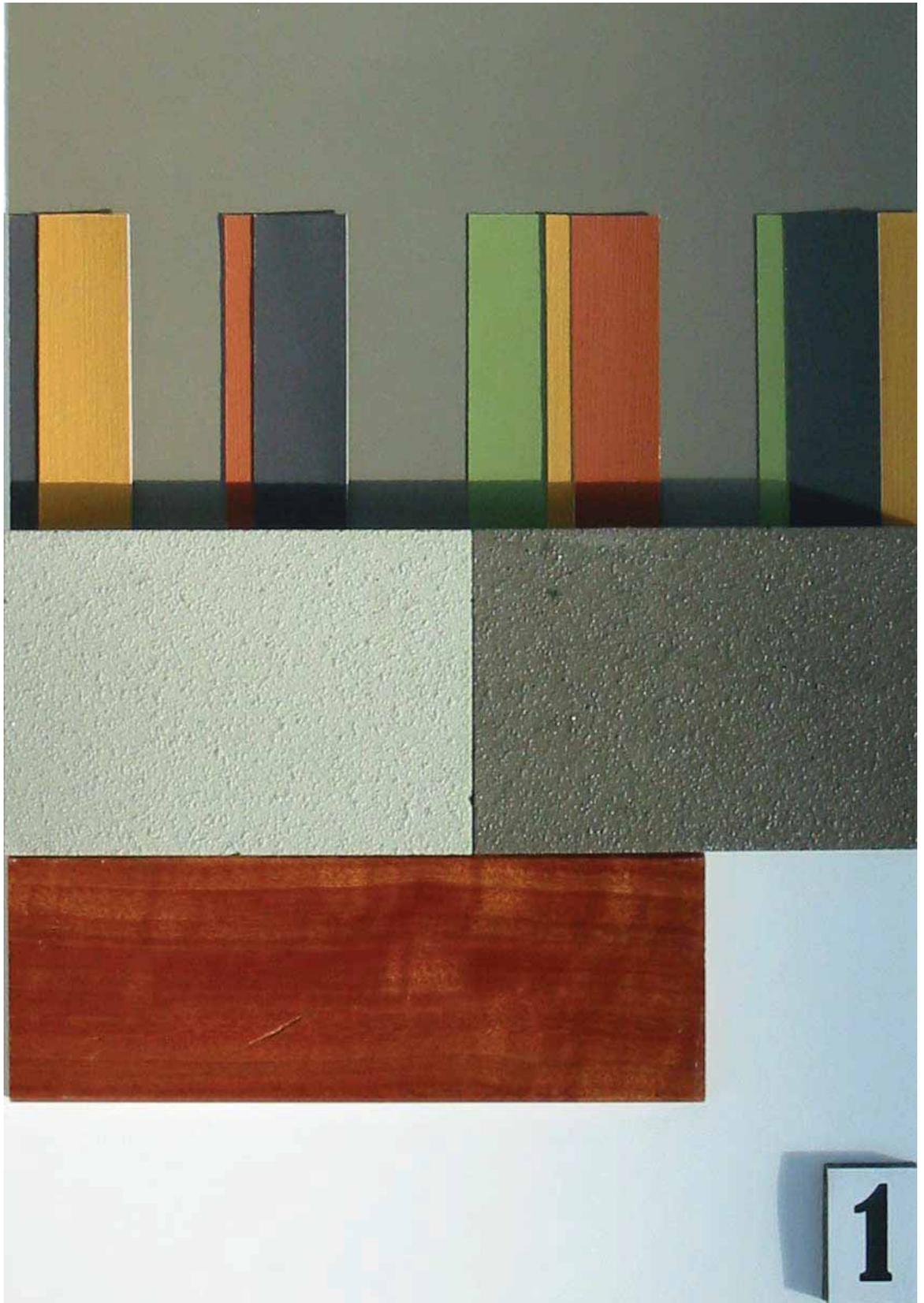
Colours & Materials Schedule: Dorset Road

Colour along Dorset Road can afford to be more adventurous and a little less controlled. This will suit the busy thoroughfare and the differing smaller scale retail / restaurant buildings.

Differentiation between the businesses at a ground floor shop front level should be encouraged, with a simplification of colour at upper levels. In the larger valley view, the little buildings of varying colours provide focus and contribute to the “village” vision.

Being a busy car route, pollution has an effect on the colours of the buildings, particularly on larger scale buildings. Subsequently the use of natural brickwork, stone or timber as opposed to painting and rendering must be used.

If repainting existing surfaces, consider dark deep-base colours in lieu of white based colours.



Wattyl 'Urban Grey' Render to larger building areas		
Wattyl 'Cumberland' - shopfronts	Porter's Paints 'Golden Ochre' - shopfronts	Porter's Paints 'Ayer's Rock' - shopfronts
Wattyl 'Cumberland' - shopfronts	Wattyl 'Monstera Leaf' - shopfronts	Porter's Paints 'Golden Ochre' - shopfronts
Porter's Paints 'Ayer's Rock' - shopfronts	Wattyl 'Monstera Leaf' - shopfronts	Porter's Paints 'Golden Ochre' - shopfronts
Sadler Stone 'Shale' Paving	Sadler Stone 'Fossil' Paving contrast	
Victorian River Redgum Furniture / air conditioning unit covers		

Dorset Road **1**

2

6.2 Boronia Village

Context

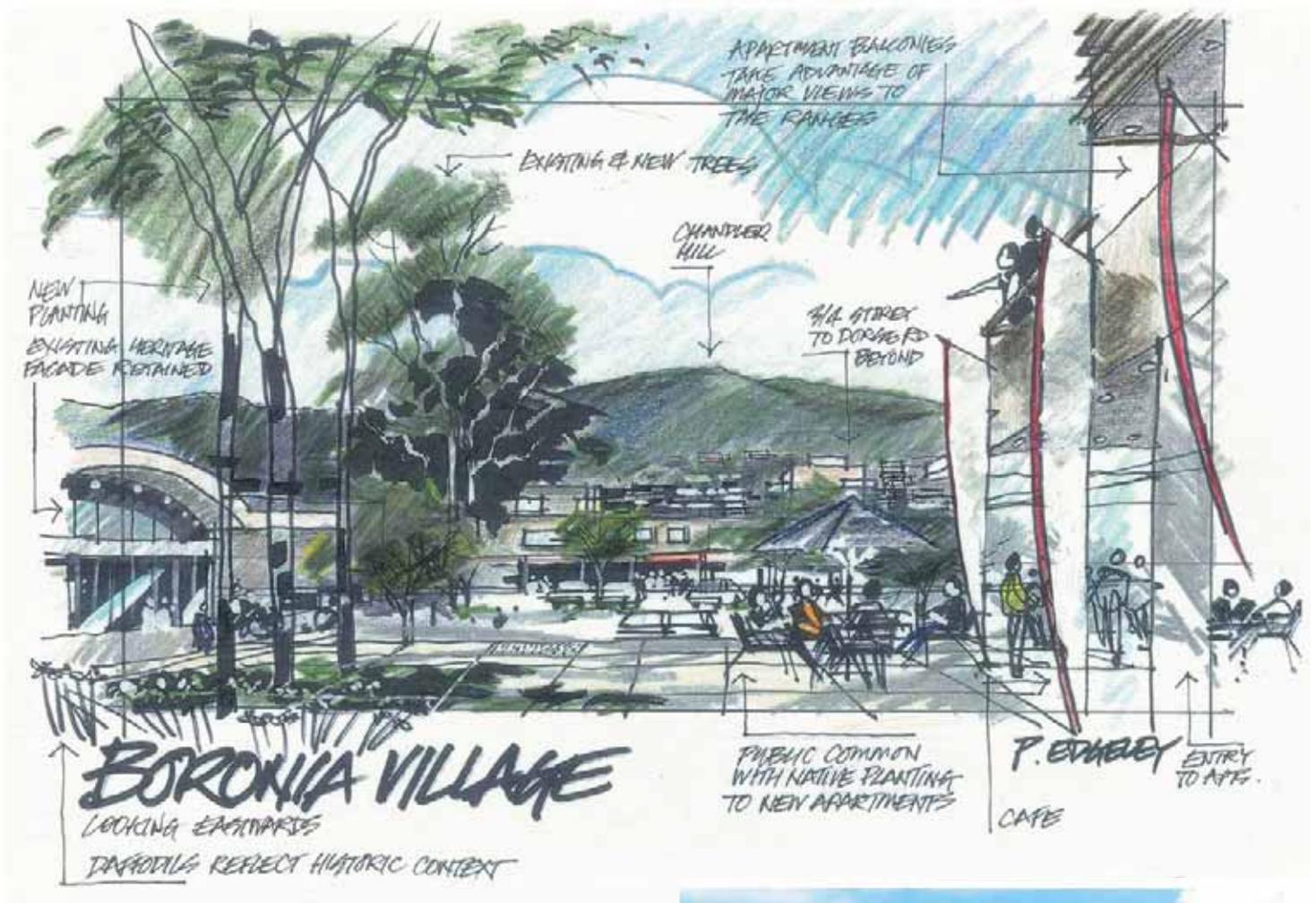
A high level of vacant premises and the least active convenience food retailing area in the Boronia Activity Centre, present significant challenges to the Village. Notwithstanding, the Village is provided with outstanding views towards the Dandenong Ranges in their entirety, is adjacent to Tormore Reserve and comprises a number of active retailers.

The Village has been identified as a location where a mixed-use ground floor retail and upper level residential development can help achieve the wider Strategic Directives of containing core retail activities.

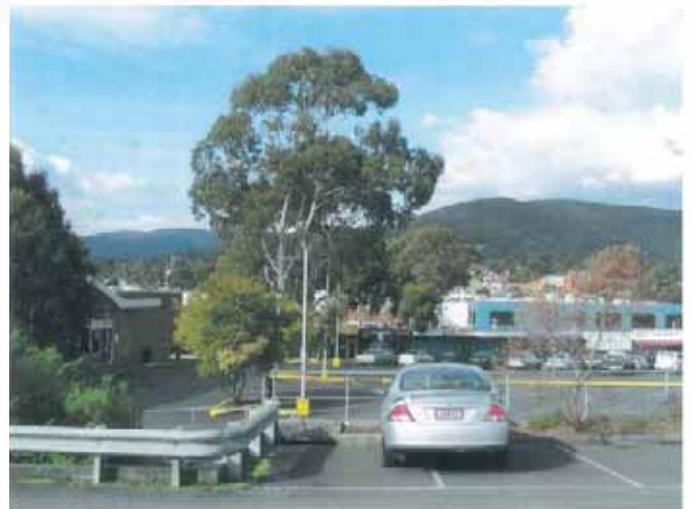
Our Future

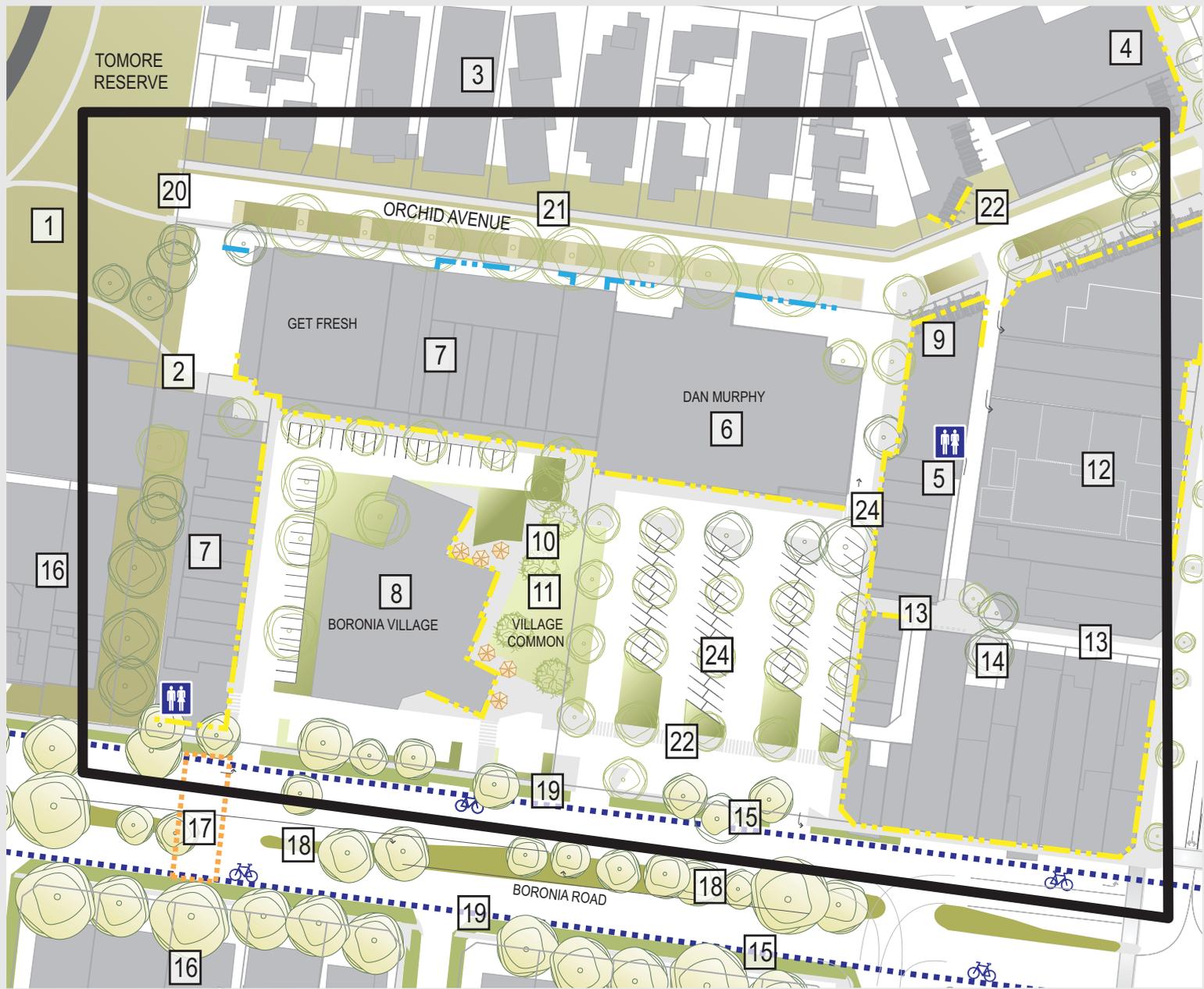
"Boronia Village is a central focal point or 'village common' for residents and visitors to Boronia. It accommodates public gatherings, small crowds, festivities and markets within a public open space area. It is a unique point within the commercial area which provides an outlook over the Ranges and Chandler Hill, where a series of innovative destination oriented businesses are located on the ground floor, and where residential and office activities share views on the upper levels towards the Ranges. Workers and residents in the Village walk directly to Tormore Reserve and Erica Avenue, along new pedestrian pathways. Pedestrian lights south across Boronia Road (between the Village and the medical tenancies) have improved access for workers on the southern side of Boronia Road into the Village, with people enjoying the open space common during their lunch hour.

New buildings and infill development have provided smart architectural forms which reflect and provide a reference to the post-war heritage attributes of adjoining development. The old Safeway building, which was one of the first to open in Melbourne, is now architecturally up lit on its façade, and has a description of its significance."



Boroonia Village future view towards the Dandenong Ranges from upper level car park (existing conditions within inset photo)





- BORONIA VILLAGE PRECINCT AREA
- OPEN SPACE
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- BIKE PATH
- PUBLIC ART / ARCHITECTURAL DETAILING
- ACTIVE / PERMEABLE BUILDING EDGE
- TOILET

- 1** Tomore Reserve: Identify opportunities for community festivals and events building on the outstanding view and proximity of the Ranges. Such festivals should build on the semi-rural character of Boronia and proximate areas, and strong cultural heritage. Investigate the potential for infrastructure which would support a multiplicity of uses of the reserve. Ensure development adjoining the reserve provides a direct address to the park and opportunity for passive surveillance. Improve pedestrian pathways, undertake avenue tree planting to define paths and provide directional signage.
- 2** Provide a pedestrian pathway to Boronia Village from Tomore Reserve. Ensure adjoining development provides active edges to the path to increase opportunities for passive surveillance.
- 3** Higher density residential development. Between buildings and the site's street frontage encourage a highly landscaped indigenous theme to contribute to the 'Green Link' between Tomore Reserve

- and the station. Building heights in this location must not obscure views to the Ranges from Tomore Reserve.
- 4** Ensure parking and access to the Busybodies facility complies with DDA standards.
- 5** Refurbish existing publicly accentuate toilet facility. Provide directional signage and ensure facility meets DDA requirements.
- 6** Recognise and protect the aesthetic and historic significance of the post-war Safeway building.
- 7** Investigate opportunities for a mixed-use retail-residential-commercial services precinct. The form and scale of development should maximise opportunities for views to the Ranges.
- 8** Investigate opportunities for a mixed-use commercial development within the upper level of the Village car-park areas. Impose a 2-storey maximum building height so as to maintain views to the Ranges for infill development to the west through a 2-storey maximum building height. Provide undercroft parking for residential use.
- 9** Mixed-use commercial-services development. Parking accessed via rear laneway.
- 10** Bike rack.
- 11** The Village Common: For any commercial redevelopment of the upper level car park provide a village common with views of the Ranges. Encourage outdoor eating and activity, farmers market, night bazaar etc.
- 12** Investigate the provision of a family services centre on upper levels within proximity of the station.
- 13** Pedestrian link: Erica Avenue - The Village. New Development to provide address to laneway. In part shared pedestrian-vehicular zone incorporating tactile surface treatment.

- 14** Retain and make feature of existing tree.
- 15** Bus stop: Improve shelter and surrounds. Ensure pedestrian access to and around shelters are DDA compliant.
- 16** Mixed-use commercial-professional services.
- 17** Future opportunity to improve connectivity between proximate commercial-professional services and the Village.
- 18** Maintain and enhance the indigenous boulevard entry along Boronia Road. Extend into the commercial area.
- 19** On-road bike lane.
- 20** Remove cyclone fence, clear vegetation and marcate the entry along Orchid Avenue into Tomore Reserve.
- 21** Between buildings and the site's street frontage encourage a highly landscaped indigenous theme to contribute to the 'Green Link' between Tomore Reserve and the Station. Within the public realm undertake landscape improvements along nature strips. The rear of commercial development to Orchid Avenue should incorporate public art and/or architectural fencing and detailing to screen development and improve perceptions of safety. Underground overhead wires where appropriate.
- 22** Provide pedestrian right of way through car park areas. Ensure the car park area is DDA compliant.
- 23** Pedestrian crossing.
- 24** Reinforce indigenous landscape canopy through lower level car park area.
- 25** Improve vehicular and pedestrian lines of sight entering And exiting the village from Orchid Avenue.

Figure 11
boronia *Your Place, Your Life, Our Future*
 Precinct Plan 2: Boronia Village
 (October 06)



Actions and Project Elements

- Undertake a detailed feasibility study of the Boronia Village mixed-use concept, in conjunction with property owners and traders. This concept should provide a direct pedestrian link between the Village and Tormore Reserve, with adjoining development providing active edges to the path. Any redevelopment of the Village should include the provision of a common with views of the Ranges, where active tenancies provide an address and where community events can be accommodated. Provide a public toilet facility within the Village area. This facility should address Crime Prevention Through Environmental Design principles.
- Within Tormore Reserve improve pedestrian pathways, provide avenue tree planting along defined paths and provide directional signage. Investigate the ability to conduct community festivals and events building on the outstanding view and proximity of the Ranges. Such festivals could incorporate the landscape character of Boronia, and the strong cultural heritage of the area. Investigate the potential for infrastructure which would support a multiplicity of uses of the reserve.

Development that adjoins the reserve must provide direct address to the park and provide opportunity for passive surveillance.

Remove cyclone fences, clear vegetation and marcate the entry along Orchid Avenue into Tormore Reserve. Landscape improvements at the entrances to the reserves must consider the opportunities for the seasonal or temporal closure of the entrances to the reserve to enable the collection of gate revenue during sporting finals.

- Facilitate increased residential density development north of Orchid Avenue. Building heights in this location must not obscure views to the Ranges from Tormore Reserve. Between buildings and the site's street frontage encourage a highly landscaped indigenous theme to contribute to the Green Link between Tormore Reserve and the station and extend this theme (within the public realm) along nature strips. The rear of commercial development along Orchid Avenue should incorporate public art and/or architectural fencing and detailing to screen development and improve perceptions of safety in this location. Underground overhead wires where appropriate.
- Undertake further review to protect the aesthetic and historic significance of the post-war Safeway building. Opportunities may be provided to communicate the heritage significance of this building to the wider community.
- The opportunity to spread economic benefit between a revitalised Village precinct and Erica Avenue, may be achieved through a direct pedestrian link between Erica Avenue and the Village. The use of a shared pedestrian-vehicular zone may require tactile surface treatment. New infill development along the link should provide address to the laneway.



Seek the long term removal and replacement of signage towers within the commercial environs.

- Investigate the opportunity for a signalised intersection to Park Crescent to facilitate connectivity between Boronia Park and the commercial area to the north.
- Provide pedestrian right of way through car park areas that provide access for all ages and abilities.
- Seek the long-term removal of the signage tower within the Village, & replacement with a horizontal business identification sign.

Colours & Materials Schedule: Boronia Village

The recent renovation of the old 'Safeway' building – retaining its natural brick colouring and friendly curved façade - sits comfortably with the future landscape and architectural visions for this area.

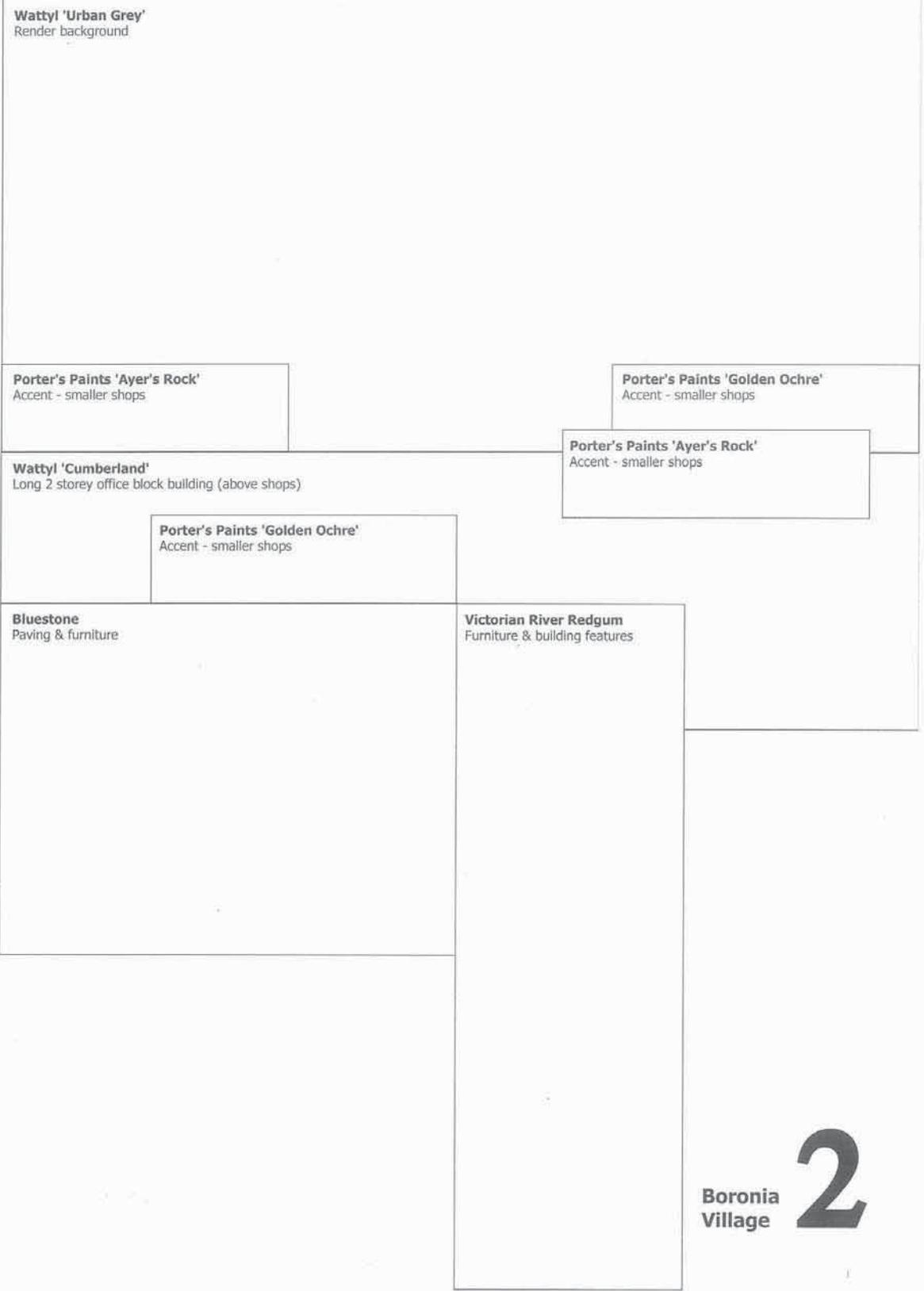
In considering the future visions for this area, the colours of the long foreground perimeter buildings are currently dominating this peaceful bush landscape view. Because this site is elevated – the eye rests on the middle ground buildings at the foot of this hill and so the colour and shielding of roof-mounted services should be considered.

Colours and materials for the buildings directly in front of the views to the ranges should be natural and earth based tones.

Consider dark neutral tones for roofing and / or timber slatted shielding of the plants. Encourage the larger scale office buildings to become a more sophisticated backdrop of natural materials and colours.

Utilize energetic colours and lighting for smaller inserted retail / restaurants / market stalls.





3

6.3 Southern Terraces

Context

Limited connections across Boronia and Dorset Road have resulted in an almost isolated node of peripheral commercial activity adjoining an established residential environment to the south-east of the Activity Centre. Indirect and infrequent points of access provide barriers to pedestrian movement across Boronia Road, and the lack of signals at the intersection of Turner Road, Torrens Avenue and Boronia Road provides an ongoing safety concern for vehicles entering and exiting Dorset Square. There is opportunity along Boronia Road, to reallocate road space through the centre for a bike path connection to the Forrest Road path.

A series of north-south streets south of Boronia Road, parallel to a fall in grade from the east to the west towards the centre, provide a series of vantage points towards the ridgeline west of the centre. This established urban form is characteristic of terraced forms of development, in which a slight intensification of building height can provide residential and commercial development with shared views to the west and over the commercial environs.

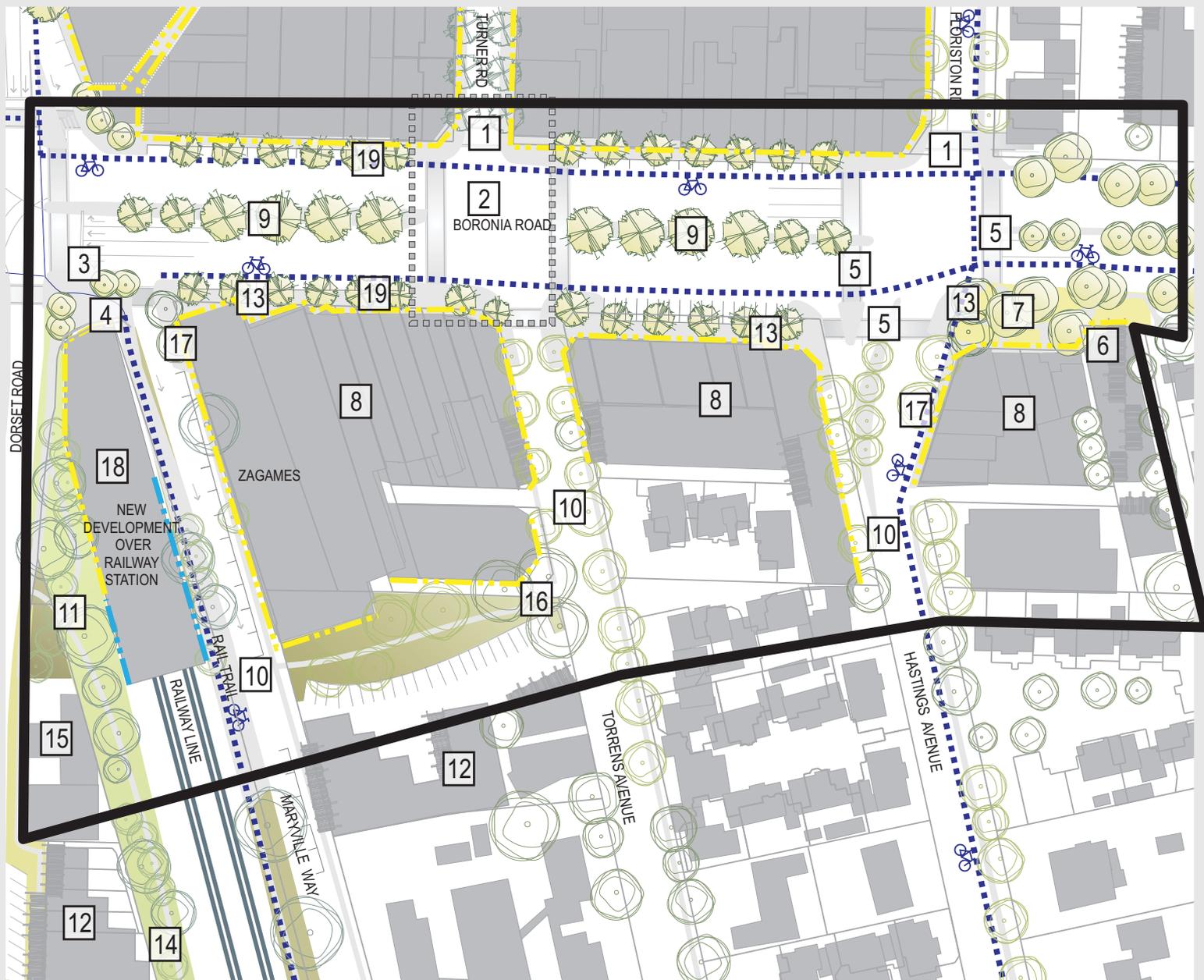
Zagames is an established night time attractor within the commercial area, and is supported by a series of restaurants and eateries proximate to the cinema complex. Many of the restaurants are not active during the day.

Our Future

"The southern terraces has become an ideal location for living with an outlook over Boronia and surrounding areas. Providing a link to the foothills environment, many indigenous canopy trees including stringy barks and indigenous grasses have been planted along streets and within the front setbacks of new development. This has resulted in the outlook from new development into the canopy of trees, and provides a habitat for many indigenous birds and fauna.

New traffic lights have improved pedestrian access to Dorset Square, and many more residents from this area now walk into the centre. Some even walk or ride their bikes along the Green Links from Ferntree Gully and the Basin or along the series of designated on-road bike paths.

The widened footpaths and canopy trees along Boronia Road have created a great location in the shade for outdoor café's and dining. During the evening the café's are a great place to meet friends before catching a movie or going bowling."



- SOUTHERN TERRACES PRECINCT AREA
- OPEN SPACE
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- BIKE PATH
- PUBLIC ART / ARCHITECTURAL DETAILING
- ACTIVE / PERMEABLE BUILDING EDGE

- 1** Provide signage along Boronia Road to direct vehicles into Dorset Square and Floriston Road deck.
- 2** Undertake further traffic reviews to determine the need for a potential signalised intersection. Include the provision of designated pedestrian crossings.
- 3** Remove slip lane and widen path to facilitate pedestrian/bike access across the Boronia/Dorset Road intersection. Remove/relocate signage poles located in direct paths of travel.
- 4** Directional signage along the bike track to designated areas of interest. Improve the definition and provision of the path through the intersection.
- 5** Provide direct and continuous accessible path of travel across Boronia Road.
- 6** Higher density residential development. Provide minimum setback to Boronia Road for key stone edge to commercial area so as to create a sense of enclosure and definition from surrounding residential development to the east.
- 7** Entry landscape treatment. Remove unusable parking spaces. Provide public seating within landscape areas and along steep pedestrian pathways.
- 8** Mixed-use commercial-professional services. Encourage upper level residential development. Provide active edge to Boronia Road.
- 9** Reinforce exotic boulevard planting along this defined section of Boronia Road. Incorporate canopy planting to provide shade and sense of enclosure. Provide feature uplighting.
- 10** Street tree planting incorporating indigenous canopy trees to provide a terraced vegetated effect and to strengthen foothills characteristics visible from the west.
- 11** Reinforce gateway urban design treatment through complimentary tree planting.
- 12** Higher density residential development: provide address to rail corridor and overlook Boronia Park.
- 13** Utilise excess road space to widen pedestrian footpath & provide onroad bike lane to Forrest Road.
- 14** Extend railway line biodiversity corridor into the centre. Possible pedestrian link as alternative pedestrian link to Dorset Road for isolated residential development to the south.
- 15** Relocate existing petrol station on approaches to the commercial centre. Ensure development provides landscape treatment to marcate entry to Boronia.
- 16** Maintain viewline through to Boronia Park to maintain the perception of proximity between areas of interest.
- 17** Bike rack.
- 18** Investigate opportunities to build over the railway line
- 19** Bus Stop.



Figure 12

boronia *Your Place, Your Life, Our Future*

Precinct Plan 3: Southern Terraces

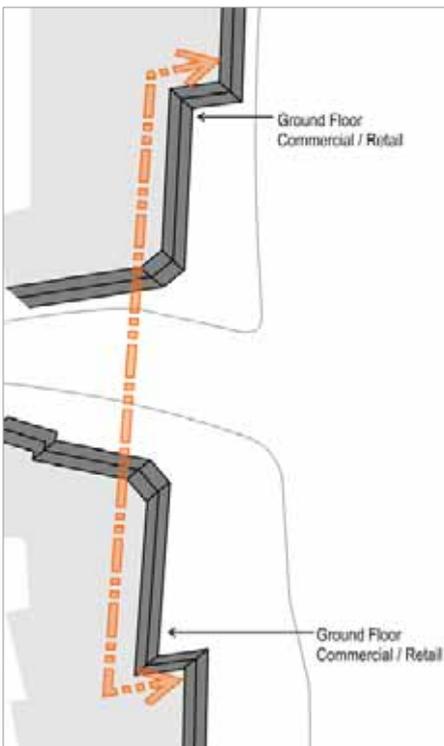
(October 06)



View west along Boronia Road
(existing conditions within inset photo)

Actions and Project Elements

- Undertake further traffic review to determine the need for improved vehicular turning and pedestrian crossing opportunities at Boronia, Torrens and Turner Roads through the establishment of traffic lights. Any works should include the provision of designated pedestrian crossings.
- Undertake entry landscape treatment at the intersection of Hastings Avenue and Boronia Road. This may be undertaken as part of any adjoining residential and commercial developments. The works may include the removal of unusable parking spaces, and must provide public seating or pause points within landscape areas and along the steep pedestrian paths.
- Provide signage along Boronia Road to direct vehicles into Dorset Square and Floriston Road to access the upper level of the Dorset Square deck.
- Provide directional signage along the bike track to designated areas of interest. Improve the definition of the path through the intersection.
- Realign pedestrian crossings to provide continuously accessible paths of travel across Boronia Road at existing intersections.



Projecting buildings towards the street can provide a sense of enclosure and definition to commercial areas

- Higher density residential development on the eastern edge of the precinct could provide a sense of enclosure and definition to the commercial environs by *projecting buildings* towards the street. Provide minimum setbacks to Boronia Road and differentiate the commercial area from surrounding residential development to the east.
- Within the commercial area encourage a mixed-use commercial–professional services precinct at ground level and encourage upper level residential development. Provide active tenancies to Boronia Road.
- Incorporate indigenous canopy planting to provide shade and a sense of enclosure along Boronia Road. Extend the 'Bush Boulevard' character from the Ranges into the commercial environment and provide feature up-lighting of landscaping.
- Along Torrens and Hastings Avenues, provide indigenous canopy trees to provide a terraced vegetated effect and to strengthen foothills characteristics visible from the west.
- Through the use of trees, reinforce the existing gateway urban design treatment at the entry to the commercial area, north of the existing 7-11 convenience store on Dorset Road. This may comprise landscaping and entry signage in addition to the existing gateway treatment.
- Facilitate higher density residential development between Dorset Road and the railway line. Extend the railway line biodiversity corridor into the centre providing a level of amenity for residents in this location, and investigate the establishment of a pedestrian link along the railway line as an alternative to the pedestrian path from Dorset Road.

-
- Utilise excess road space along Boronia Road to widen the pedestrian footpath and provide an on-road bike lane to Forest Road.
 - The redevelopment of the Zagames site must maintain the view line through to Boronia Park from Torrens Avenue. This will enhance the perception of proximity between areas of interest.

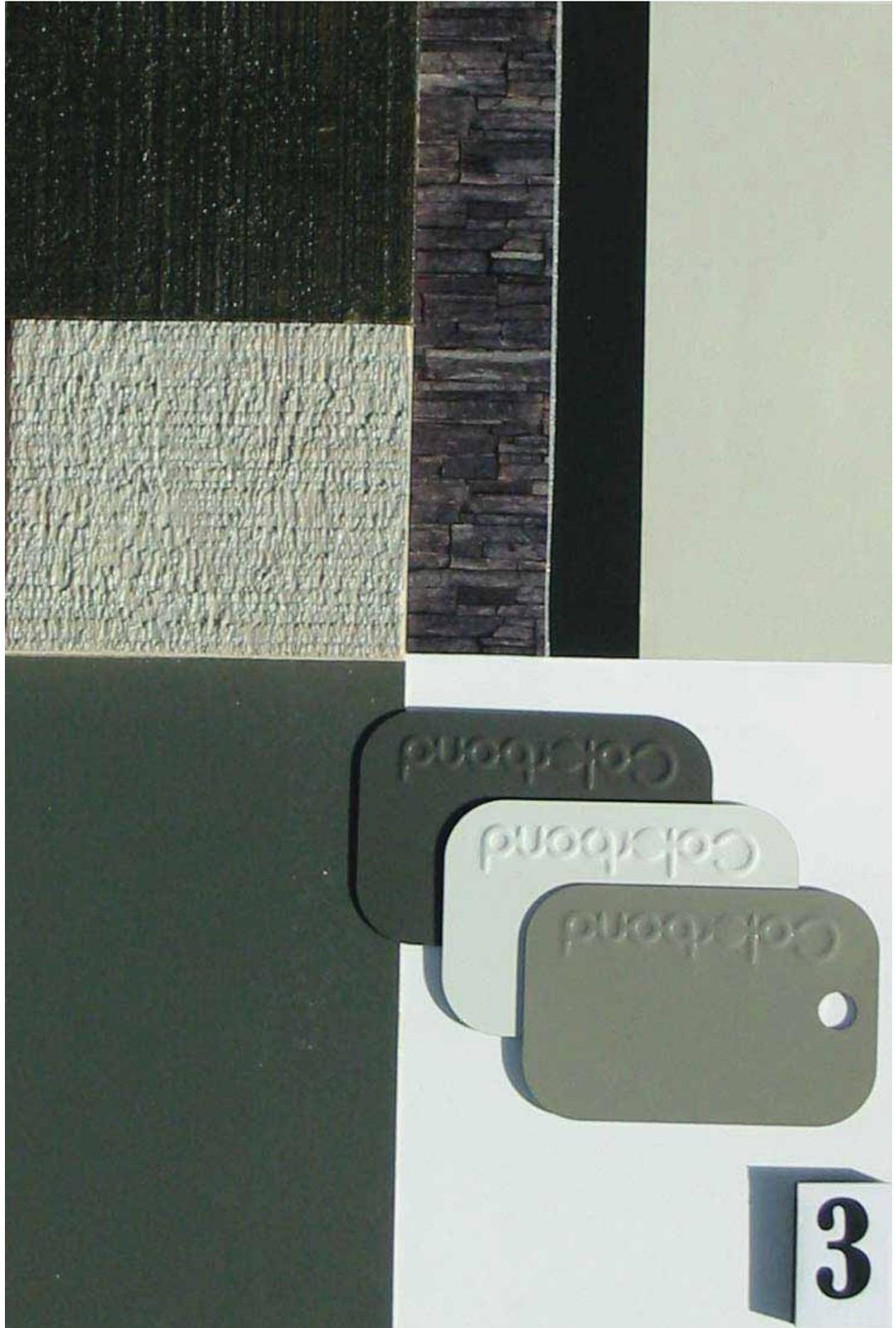
Colours & Materials Schedule: Southern Terraces

When driving west from the Ranges, currently, this commercial precinct is a very sudden introduction to the open suburban landscape of Boronia.

In keeping with the theme to extending the *Bush Boulevard* character from the Dandenong Ranges there is an opportunity for more natural, robust materials in any new development planned for this commercial precinct, and proximate new development.

In line with the extension of the native landscape, proposed colours aim to 'blur' the boundary with a warmer natural junction between the bush and the commercial area.

Future development in this area, should incorporate natural robust, textured materials such as stacked stone and timber to assist in defining this uniquely Boronia mix of commercial with the natural environment.



<p>Woodmans colour no.32 'Midnight' Apartments - timber facade 1</p>	<p>Stacked stone Features, landscape, building facade</p>	<p>Dulux 'Black Onyx' 00NN 07/000 Highlights</p>	<p>Wattyl 'Chino' Main colour 1 - rendered facades</p>
<p>Woodmans colour no.18 'Driftwood' Apartments - timber facade 1</p>			

Wattyl 'Woodland Grey'
Main colour 2 - fences & facades

Colourbond 'Woodland Grey'
Roofing metal 1

Colourbond 'Surfmist'
Roofing metal 2

Colourbond 'Bushland'
Roofing metal 3

Southern Terraces 3

4

6.4 Boronia Junction

Context

Boronia Junction is characterised as a freestanding car-based retail development adjacent to the station and transit interchange area. The Junction development presents a blank building edge to Erica Avenue, while the setback of retail activity from Dorset Road further isolates the Junction from adjoining retail areas. The petrol station, substandard layout of the car park and poor pedestrian pathways cause congestion.

Erica Avenue is a service orientated commercial strip. The pedestrian scale of the street width and the scale of adjoining development provide a comfortable and contained retail environment for pedestrians. Visual cues indicating the proximity of Boronia Park and the Village area are limited, and there are no direct pedestrian connections to these areas of interest.

Within immediate proximity of the station and interchange area, numerous VicTrack sites are underutilised, and currently provide at grade car parking. The redevelopment of these sites, including the airspace over the bus interchange and connex car park would boost the level of activity, and increase opportunities for passive surveillance within, over and around the station environs.

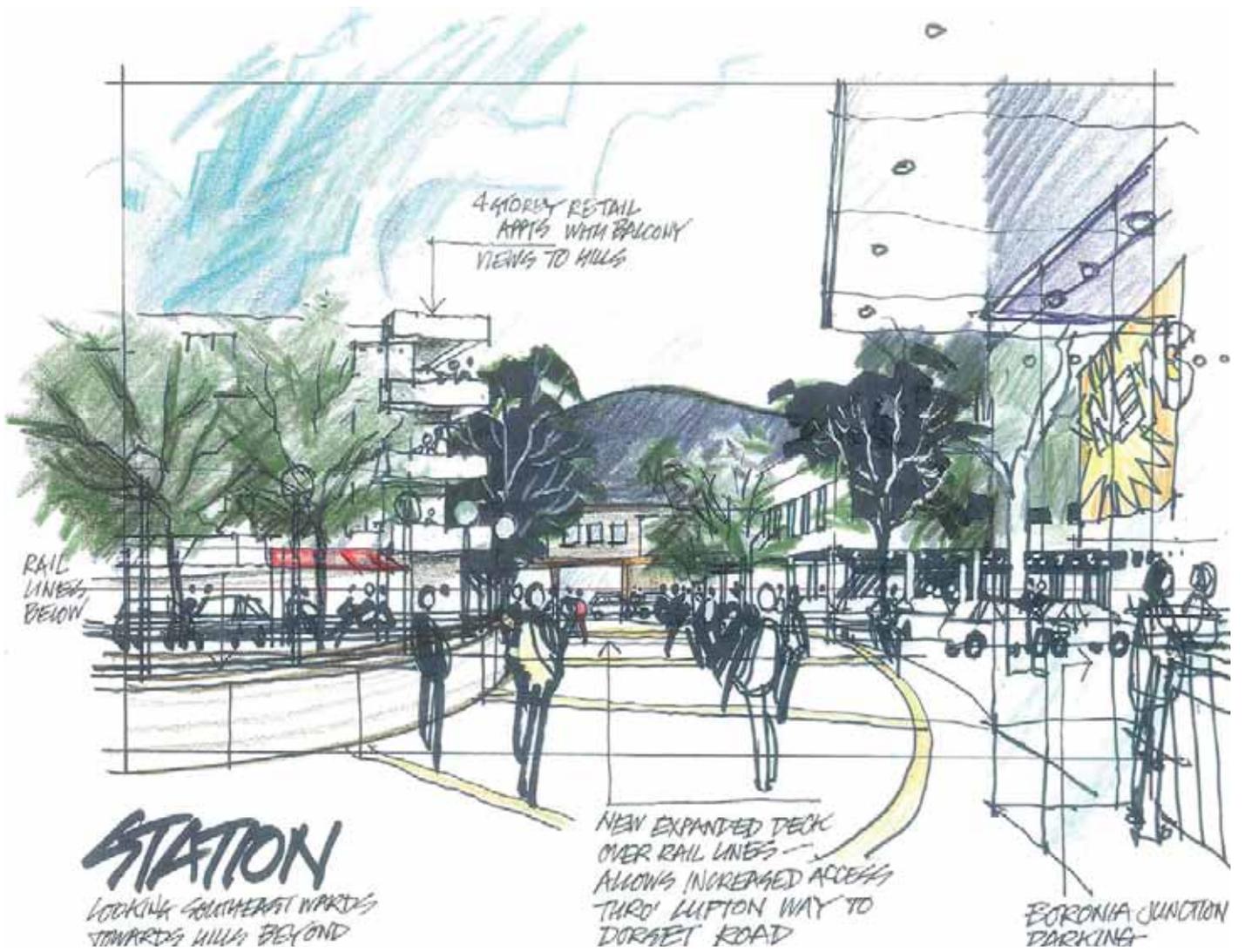
The park adjacent to the station has become underutilised and is perceived to be an unsafe children's playground due to the inactive edges and poor surveillance surrounding the park. The park has become renowned as a location for 'undesirable' activities.

Our Future

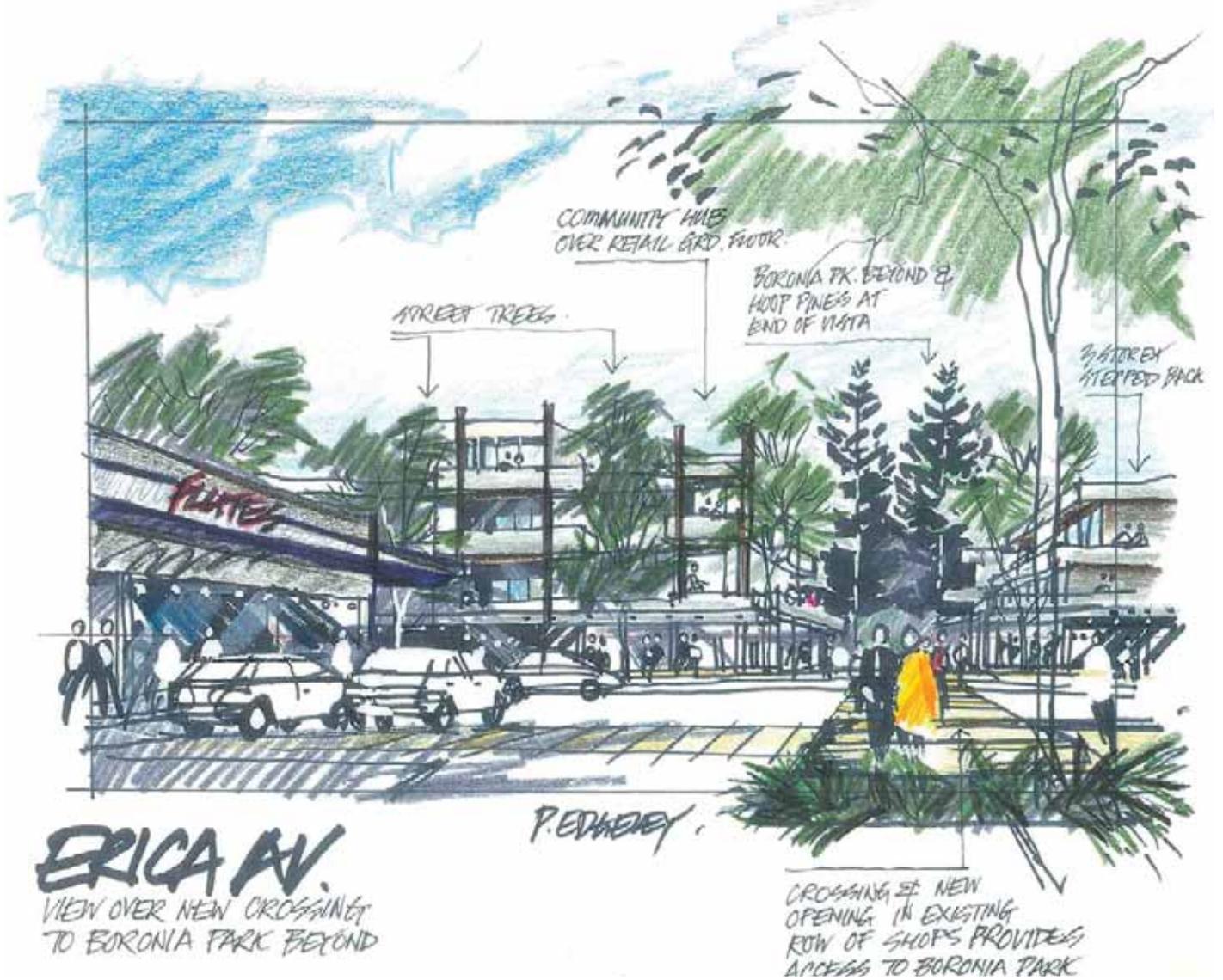
"The station and transport interchange is a vibrant and bustling centre of activity providing a principal point of access to the Boronia Activity Centre. It is surrounded by work places and housing types which rely on the nearby public transport facilities. From the station area, directional signs and visual cues in the urban environment enable visitors (particularly those who are visiting Boronia for the first time) to easily find their way around the centre.

Landscaping, pedestrian prioritisation and urban design works around the station have created a comfortable pedestrian environment where there is ample shade and places to wait. A number of retail tenancies now face the station area, offering the opportunity to grab the paper and a quick bite on the way to work. There are no longer concerns with safety (even on the station platform) as upper level residential development overlooks the area.

Erica Avenue has become an important service retail environment. It is now a critical link, providing safe and direct pedestrian connections between Boronia Park, the Village, the Junction and the station. The street tree planting and the newly opened view to Boronia Park from the west side of the street has created a quiet, comfortable street, which at times is closed to provide an area for community festivals and events."



View across the Boronia Station environs (existing conditions within inset photo)



View south from Erica Avenue through new pedestrian walkway to Boronia Park (existing conditions within inset photo)

-  BORONIA JUNCTION PRECINCT AREA
-  OPEN SPACE
-  PEDESTRIAN SPACE SYSTEM
-  BUILDING FOOTPRINT
-  BIKE PATH
-  PUBLIC ART / ARCHITECTURAL DETAILING
-  ACTIVE / PERMEABLE BUILDING EDGE

-  TAXI RANK
-  LOADING ZONE
-  TOILET

- 1 Pedestrian crossing: provide direct pedestrian routes between the station and adjoining areas of interest. Priority should be given to improving pedestrian links from the interchange to Tomore Reserve through continuous paths of travel-crossing points, and landscaped green links.
- 2 Taxi rank-at the junction should be widened and appropriately signed.
- 3 Encourage development around the station area incorporating active frontages towards the interchange to address security issues. Provide undercroft parking and/or off-street commuter parking.
- 4 Urban Design improvements to the station area to provide an address and sense of enclosure. Ensure sufficient levels of lighting around the interchange and pathways to the station.
- 5 Bike rack/storage facility.
- 6 Improved pedestrian link between Erica Avenue & Boronia Junction. Incorporate lighting/art work. Provide a continuously accessible path of travel and improved lines of sight between areas of interest.
- 7 Relocate or redesign utility box/street light around pedestrian pathways.
- 8 Car park / potential upper level office development: Provide tactile ground surface indicators along pedestrian routes and marcate the end of the pedestrian pathway leading to the upper level car park.
- 9 Principal Pedestrian Link: Extend the deck over the railway line to facilitate a more direct pedestrian link between Orchid Avenue (link to Tomore Reserve), Lupton Way across Dorset Road. Improve sight lines to the station platform from upper level areas which may include the use of permeable barriers/balustrades. Provide a high quality urban landscape incorporating local art.
- 10 Provide prominent directional signage to car park points of entry from Erica Avenue and improve sightlines for vehicles and pedestrians exiting this structure.
- 11 Erica Avenue: Incorporate substantial short-term and long-term parking to meet the needs of both the commercial premises and to accommodate commuter parking where possible. There is opportunity to incorporate substantial canopy tree planting to reinforce the pedestrian scale and sense of containment in this street. Increasing pedestrian movement (commuter parking within the street and within Boronia Park) will improve the outlook for active tenancies particularly on the west side of Erica Avenue. Temporal street closure provides a potential location for community festivals and events within the commercial environs.
- 12 Increase the width and dimensions of car parks. Improve pedestrian facilities and connectivity in car park areas, through defined pedestrian right of way paths. Approximately 185 parking spaces (currently 190). Incorporate water sensitive urban design in layout of new car park.
- 13 Artwork/detailing to fence. Canopy trees to be provided in landscape bumpout.
- 14 Seek continuance of pedestrian path. Will require acquisition of existing car parks under private ownership. Seek the long term maintenance and replacement of existing eucalyptus to maintain substantial canopy spread and screening of new development.
- 15 Investigate the provision of a family services centre on upper levels within proximity of the station.
- 16 Pedestrian link: Erica Avenue - The Village. New Development to provide address to laneway. In part shared pedestrian/vehicle zone.
- 17 Potential future pedestrian connection between evening commercial attractors (bowling centre and the cinema complex).
- 18 Provide substantial planting to screen inactive building edge. Encourage bowling centre to provide an address to Boronia Road (e.g. potential upper level function room). Seek removal of excessive signage towers.
- 19 Pedestrian crossing point on west side of intersection, providing direct access to Boronia Park. Provide two lane exit and single lane entry to improve Southbound Exit from Erica Avenue.

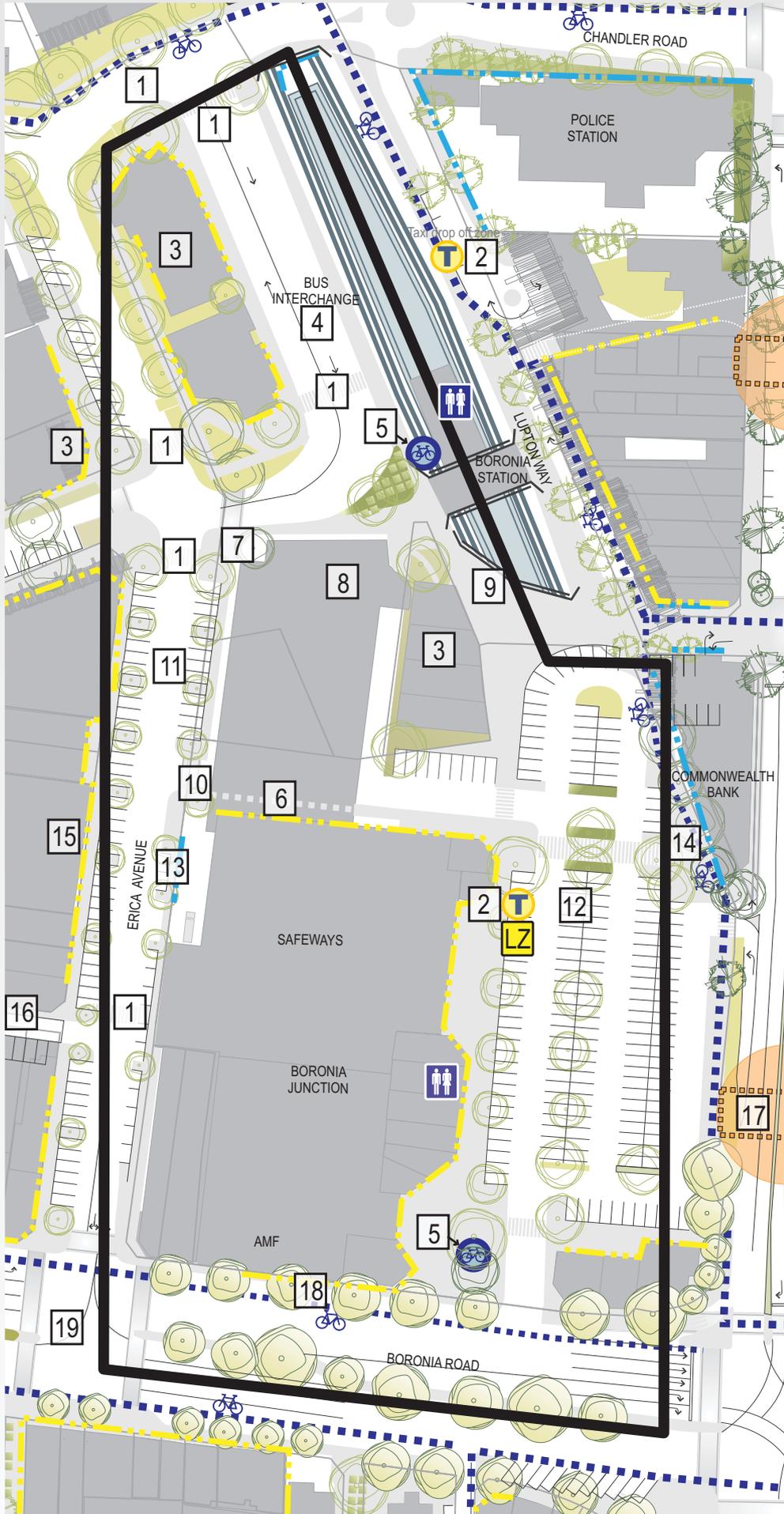


Figure 13

boronia *Your Place, Your Life, Our Future*

Precinct Plan 4: Boronia Junction

(October 06)

Actions and Project Elements

- Public works will provide direct pedestrian routes between the station and adjoining areas of interest. Priority should be given to improving pedestrian links from the interchange to Tormore Reserve through continuous paths of travel/pedestrian crossings and landscaped Green Links.
- As part of the development of the Principal Pedestrian Link, investigate the extension of the deck over the railway line to facilitate a more direct pedestrian link between Orchid Avenue (link to Tormore Reserve), Lupton Way and across Dorset Road. Any works should improve sight lines to the station platform from upper levels. This may include the use of visually permeable barriers or balustrades. Provide a high quality, *hard urban landscape* incorporating local art.
- In consultation with VicTrack, use the Boronia Park car park for overflow commuter parking, so as to allow development around the station area. Developments should incorporate active frontages towards the interchange to provide opportunities for passive surveillance, and incorporate undercroft parking. As part of any redevelopment investigate the provision of a family services centre on upper levels within proximity of the station.
- In partnership with VicTrack undertake a feasibility study of the redevelopment of the Connex deck car park to incorporate an upper level office development and the establishment of alternative parking facilities.
- Undertake urban design improvements to the station area to provide a sense of enclosure. Ensure sufficient levels of lighting around the interchange and pathways to the station.
- Provide an improved pedestrian connection through the deck car park between Erica Avenue and Boronia Junction. The link through the deck structure will incorporate lighting and artwork. Provide a continuously accessible path of travel and improved lines of sight between areas of interest.
- Address the missing link in the Principal Bicycle Network along the Belgrave Railway line (link lost in the undergrounding of the railway line and redevelopment of the airspace for the Junction development). Seek the provision of an appropriate two way, defined bike path provided along the frontage of Boronia Junction and at the rear of the new Commonwealth Bank. This will require the reconfiguration of the Boronia Junction car park area. If this is not achievable, Council in consultation with VicRoads, should seek the removal of the southbound right hand turn lane to enable the construction of a defined bike path along Dorset Road.
- Relocate (or redesign) utility boxes, streetlights and poles outside of direct pedestrian pathways.
- In partnership with VicTrack undertake a feasibility study of the redevelopment of the deck car park to incorporate an upper level office development. In the interim, provide tactile ground surface indicators along



Hard urban landscapes are places, such as squares, streets or plaza's, that contain limited trees and vegetation. Around the station platform & existing deck over the railway line, opportunity to plant trees and vegetation is limited.

The environment of these areas can be significantly improved by creating visual interest through detail within the pavement, small scale planting, unique street furniture, lighting and public art.



pedestrian routes to marcate the end of the pedestrian pathway leading to the upper level car park areas.

- Erica Avenue: Incorporate substantial short and long-term parking to meet the needs of the commercial premises and to accommodate commuter parking where possible. There is opportunity to incorporate substantial canopy tree planting to reinforce the pedestrian scale and sense of containment that exists in this street. Increasing pedestrian movement (commuter parking in the street and within Boronia Park) will improve the outlook for active tenancies particularly on the west side of Erica Avenue.

Occasional street closure of Erica Avenue may provide a potential location for community festivals and events within the commercial environs, subject to the preparation of a traffic management plan.

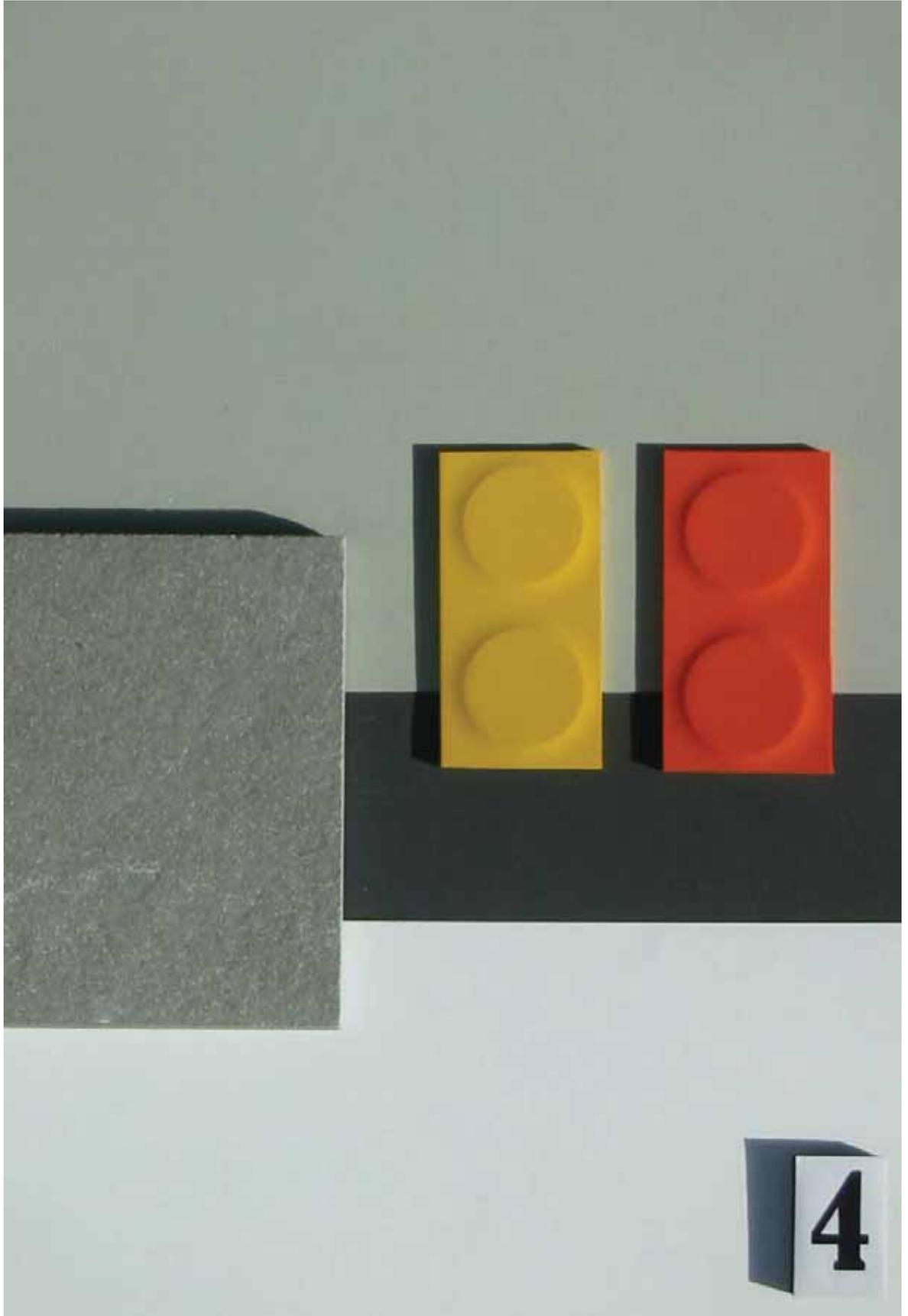
- In partnership with the owners of Boronia Junction, simplify the layout and increase the width and dimensions of car parks. Improve pedestrian connectivity with the introduction of defined pedestrian right of way paths through car park areas. Incorporate water sensitive urban design in layout of new car park.
- Encourage the bowling centre to provide an address to Boronia Road (e.g. an upper level function room).
- Seek the long-term removal of the signage tower at the junction.

Colours & Materials Schedule: Boronia Junction

Being the entry or departure point of Boronia via public transport - the structure plan's aim is to increase the retail and pedestrian activity in this area. Also, as the 'between-space' of the Boronia Village and Dorset Square shopping precincts – the linking pedestrian paths are to be given priority.

Colours for this area could be more active and dynamic on walkway surfaces. Shadow and light will encourage movement.

Colours for fencing, gardens, bin enclosures, bollards and light posts are currently inconsistent and distracting. Standard assets (such as bins, seating, and fencing) in Boronia should be united in a dark charcoal paint or 'ironstone' colourbond so as to recess their impact and to be less dominant in the natural landscape setting. This will allow the paving surface to be a dominant line making way-finding easier.



Bristol 'Kosiuskco' CW264
Concrete colouring as main render (except Safeway)

Concrete paving

Yellow tactile dots
Paving direction

Orange tactile dots
Paving direction

Wattyl 'Cumberland'
All City of Knox metal work
ie. bins / fencing / posts / bollards

Boronia Junction **4**

5

6.5 Dorset Square

Context

Dorset Square encompasses much of the current retail core of the Boronia Activity Centre. It includes the Boronia Mall, a discount department store, supermarket and cinema complex. Fragmented ownership in the arcades and the Mall has resulted in sites receiving limited reinvestment into building and store frontages over the last 20 years.

The open air car park in Dorset Square is not designed to Australian Standards, and presents an ongoing issue in the efficiency and safety of vehicular movement. The location of taxi ranks and disabled parking spaces has presented safety concerns for users. The toilet block and compactor facility in the centre of the car park have presented ongoing maintenance problems, and hamper movement through the car park area.

The substantial deck car parking structure has a low level of amenity and is generally perceived as an undesirable and unsafe car parking option in the centre. Notwithstanding, the deck has significant potential for increased utilisation thereby increasing pedestrian presence in the mall.

Dorset Square and the tenancies within the square have limited exposure and presence along main road approach routes. This has diminished opportunities for the area to draw on exposure to through traffic along Dorset and Boronia Roads.

Confusion over pedestrian right of way, limited shade, a lack of containment within the square, extensive areas of inactive building edges, poor paving and a lack of seating and pause points, have created a poor quality pedestrian environment.

Connectivity between Dorset Square and Chandler and Floriston Roads are limited, and the inactive services edge provides a bland outlook for development adjoining the rear of the Mall.

Our Future

"The redevelopment of the Boronia Mall has revitalised the Dorset Square precinct. There is increased retailing in the centre, including bulky and household goods, which now allows residents within Boronia's catchment area to shop locally. The redevelopment has an upper level with retail outlets and cafés which overlook Dorset Square and enjoy distant views south towards the Lysterfield Hills. The design of the redevelopment has incorporated aspects of the original 'Shopping Town', and has made a feature of the unique tile mosaic.

The upper level redevelopment has convenient access to the at grade level of Dorset Square, which makes it easy to do all your shopping in one location. The local retailers in Dorset Square have benefited from the redevelopment, and there are many more shops and tenancies including clothing and like products.

A new pedestrian link from Elliot Street enables residents to walk directly onto the upper level of the revitalised mall while an lift provides direct access to the Dorset Square area.

A multi-use play facility, which is used by adults and kids alike, has been built in the Square. A substantial area of grass and deciduous trees provides a great spot in the middle of this busy commercial area to relax and watch activities.

View across new open space within the Dorset Square car park & upper level redevelopment of the Boronia Mall (existing conditions within inset photo)



The car park has been resurfaced and is now easy to park in. Priority is given to pedestrians through the car park area and traffic calming and tactile road surfaces have reduced traffic speed.

The southern edge of the car park around the cinema has become a desirable destination during the evening. A series of complimentary tenancies have provided a focus for night-time activity, particularly around Cinema Lane, where the lighting display onto buildings encourages discussion about local art. A bookshop and café have opened up onto the laneway, and they provide a great place to meet friends and family in the open air. This place is now so popular they close off the southern edge of the car park to vehicles on Friday nights, to enable activity to spill out onto the Square."



BORONIA MALL
LOOKING NORTHWARDS



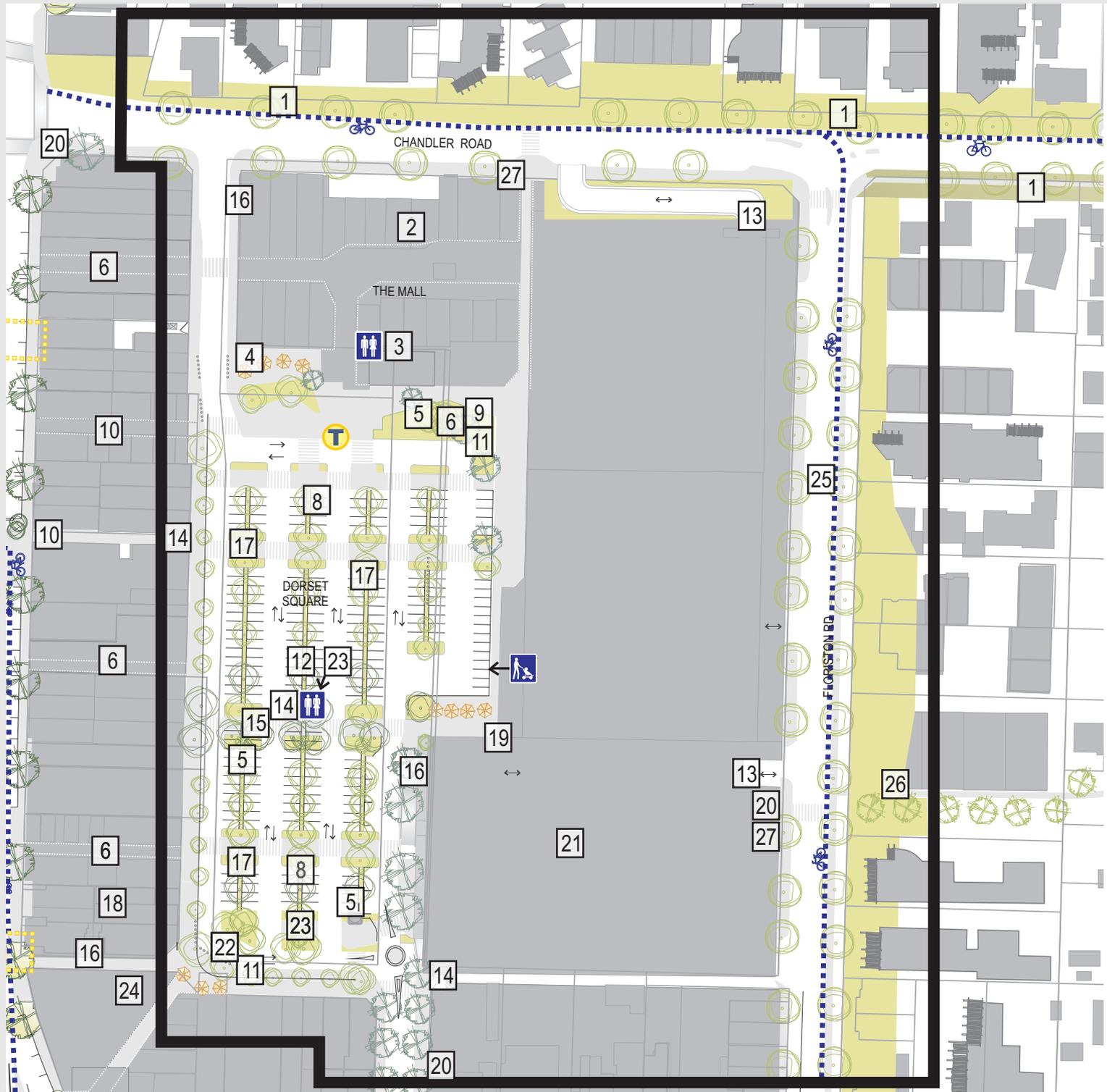


Landscape works within Dorset Square south
(existing conditions within inset photo)

Actions and Project Elements

- Two options have been prepared for the Dorset Square at grade car park area. These options recommend revising the current parking layout to provide improved circulation and access to bays (wider spaces and aisles to Australian Standards). The options will provide approximately 310-320 car parking spaces (currently 410 car parking spaces).
- Both options will provide disabled parking to a minimum of 2% of overall supply, and adjacent to walkways and pedestrian paths. A taxi rank will be located adjacent to the pedestrian pathway. Provide frequent locations for trolley bays throughout the extent Dorset Square car park. The works within





DORSET SQUARE PROJECT AREA

- OPEN SPACE
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT

- T TAXI
- ♿ TOILET
- 👶 PARENTS WITH PRAM PARKING
- 1 "Green Line" walkway linking Boronia directly with the Dandenong Ranges National Park.
- 2 Improve access in older style commercial buildings and ensure DDA compliance. Future redevelopment of the Mall should ensure continuous paths of travel be incorporated into any future design. Ensure heritage aspects of the mall including the mural, are protected and featured in future buildings and works.
- 3 Open the Mall onto Dorset Square through an active building

Figure 14

boronia *Your Place, Your Life, Our Future*

Precinct Plan 5(a) - Dorset Square (Ground Level)

(February 06)



edge.

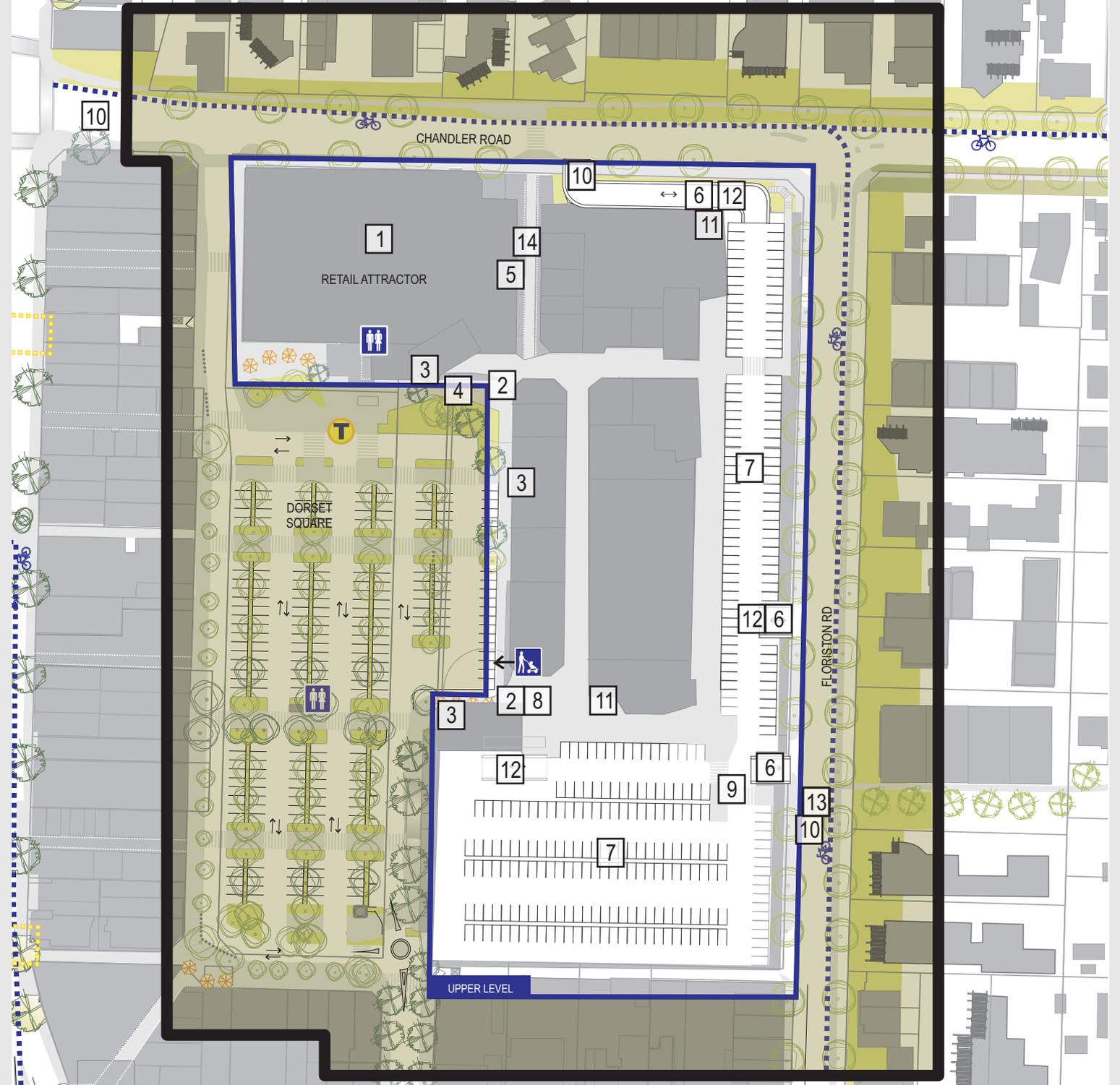
- 4 Minimise variance of grade. Protect pedestrian space with bollards to enable on-street dining protected from vehicle overrun of the pavement.
- 5 Provide disabled parking to 2% minimum of overall supply, adjacent to walkways and pedestrian paths.
- 6 Refurbish/redevelop arcades between Dorset Road/Dorset Square with adequate lighting and active frontages. Consolidate sites to ensure minimum store dimensions accommodate sustainable retail tenancies. Provide minimum floor space areas of 300m² per tenancy.
- 7 Investigate the provision of a community bus service to surrounding residential areas (particularly for elderly) providing direct access to Dorset Square.
- 8 Dorset Square car park: provide through car parking around the cinema complex. Clearly define and prioritise pedestrian movements. Revise current parking layout to facilitate improved circulation and access to bays (wider spaces and aisles to Australian Standards).
- 9 Children's multi-use play facility. Provide enclosed area.
- 10 Active edge and provide 24-hour access to the Station/Dorset Square at all times of the day and night along the "Principal Pedestrian Pathway"
- 11 Bike rack/storage facilities.
- 12 Relocate existing toilet facility and provide pedestrian scale of lighting in car park areas.

- 13 Direct vehicular access to upper level retail redevelopment of Boronia Mall.
- 14 Investigate the replacement of the compactor with recycle bins. Locate bins provided with a minimal visual presence, away from active retail edge.
- 15 Formal raised pedestrian crossing to provide central pedestrian access through Dorset Square. Consider design which will provide protection from the elements. Remove/relocate poles/lights that obstruct the direct path of travel.
- 16 Provide public art display space to showcase local artists/youth art.
- 17 Incorporate best practice water sensitive urban design in the layout of parking areas.
- 18 Work with traders and property owners to improve access in commercial buildings.
- 19 Provide lift to upper level car park area. Ensure visibility between the ramp at grade and deck car park areas are maximised. Define/mark pedestrian pathways between Dorset Square and Floriston Road.
- 20 Provide prominent directional signage to car park points of entry.
- 21 Improve lighting and surfaces within deck structure. Incorporate public art/detailing along defined pedestrian pathways. Relocate specialty tenancies to open northern edge of car park (potential to incorporate west of Coles/Kmart). Improve levels of lighting, appearance and amenity. Investigate (at lower levels) the provision of windows through to adjoining retail tenancies (Coles/Kmart).
- 22 Outside of peak time- encourage the temporal use of the car park for other activities. Use bollards to direct/restrict traffic providing wider spaces for on-street activity (extension of activity around the cinema), markets and informal recreation.
- 23 New single use toilet facility. Best practice ESD practices in the design of a new toilet facility. Incorporate educational boards to explain environmental aspects. Maximise opportunities for water runoff (from Dorset Square/adjoining buildings and structures), and solar access.
- 24 Investigate the provision of an upper level drop-in youth centre and an outlook over Dorset Square.
- 25 Underground power lines and undertake avenue planting along Floriston Road and Chandler Avenue ring road.
- 26 Provide a direct pedestrian link between Elliot Street, Floriston Road and Dorset Square.
- 27 Directional sign to shopping area.

Approximately 290 car park spaces



Knox City Council



- DORSET SQUARE PROJECT AREA
- OPEN SPACE
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- TAXI
- TOILET
- PARENTS WITH PRAM PARKING

- 1 Investigate the opportunities for the incorporation of bulky goods retail on the upper level of the Mall.
- 2 Provide direct access via lift / elevators to at grade areas within Dorset Square to facilitate linked retail trips and spread economic benefit through the centre. Ensure access for prams and limited mobility.
- 3 Upper level development should overlook and provide an address to Dorset Square. This may include an ancillary upper level café's, which overlook the square and which provide direct views to the Ranges adjacent to main retail attractor.
- 4 Open the Mall onto Dorset Square through an active building edge.
- 5 Ensure heritage aspects of the mall including the mural, are protected and featured in future buildings and works.
- 6 Direct vehicular access to upper level retail redevelopment of Boronia Mall.
- 7 Parking should be provided at a minimum ratio of 1 car parking space per 100m² retail floor space area. Improve lighting and surfaces of deck structure.
- 8 Provide lift) to upper level car park area. Ensure visibility between the ramp at grade and deck car park areas are maximised.
- 9 Define/mark pedestrian pathways between Dorset Square and Floriston Road.

- 10 Provide prominent directional signage to car park points of entry.
- 11 Building form and height should seek to maintain viewlines to the Ranges (refer "Structure Plan: Built Form (Building Height) Framework Plan").
- 12 Minimise the gradient of vehicular ramps where possible.
- 13 New pedestrian crossing providing access to Elliot Street pathway.
- 14 Improved pedestrian link to Chandler Avenue, allowing 24-hour access.
- 15 Potential to increase open space allowing car parks of upper level retail development.

Note: Provide directional sign through car park

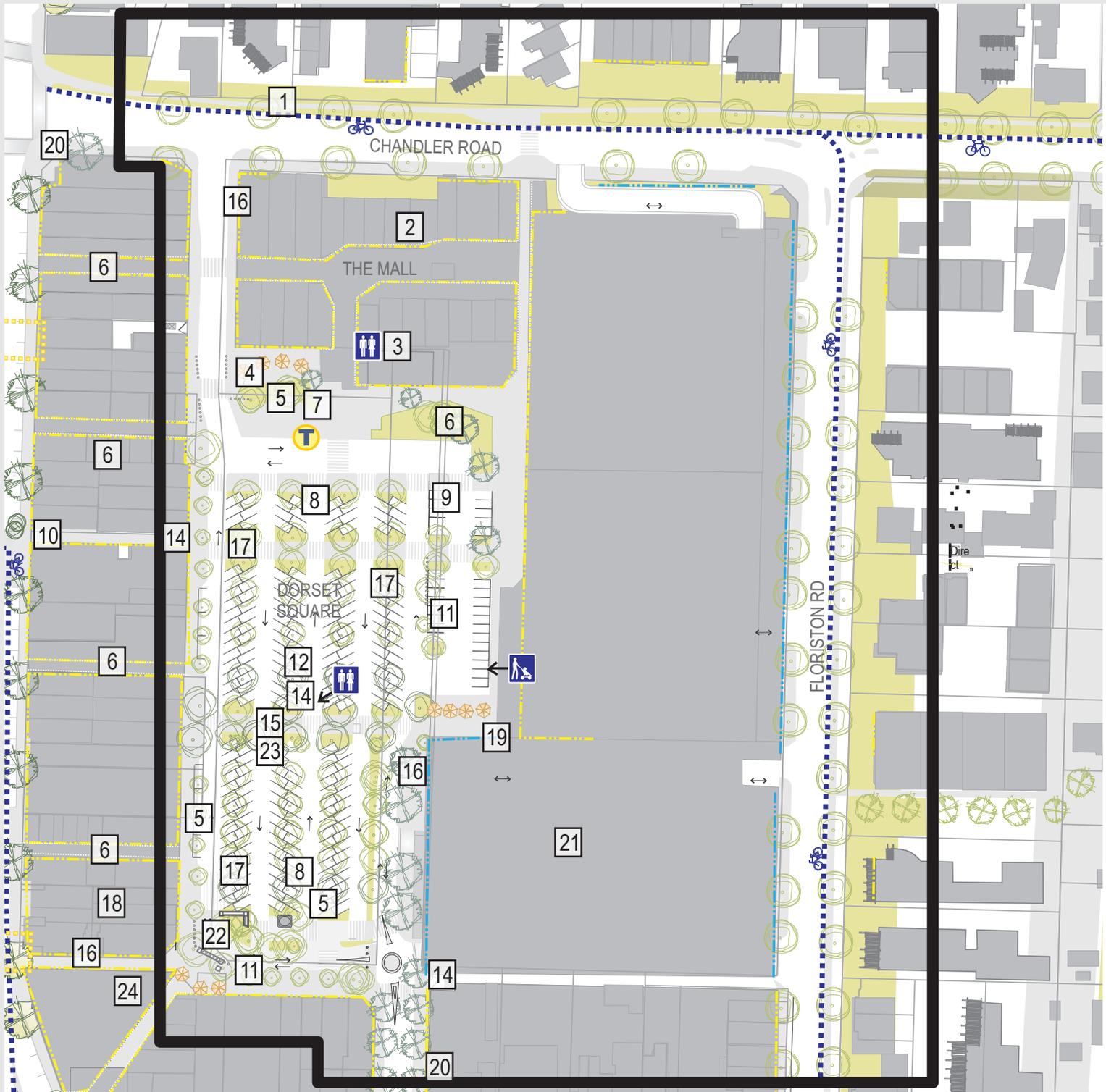
Figure 14(a)

boronia *Your Place, Your Life, Our Future*

Precinct Plan 5(a)i - Dorset Square (Upper Level)

(October 06)





DORSET SQUARE PROJECT AREA
 OPEN SPACE
 PEDESTRIAN SPACE SYSTEM
 BUILDING FOOTPRINT

TAXI
 TOILET

- 1 "Green Line" walkway linking Boronia directly with the Dandenong Ranges National Park.
- 2 Work with traders and property owners to improve access in older style commercial buildings. Future redevelopment of the Mall should ensure continuous paths of travel are incorporated into future design. Ensure heritage aspects of the mall including the mural, are protected and featured in future buildings and works.
- 3 Open the Mall onto Dorset Square through an active building edge.

- 4 Minimise variance of grade. Protect pedestrian space through bollards to enable on street dining, protected from vehicle overrun of the pavement.
- 5 Provide disabled parking to 2% of overall supply adjacent to walkways & pedestrian paths.
- 6 Refurbish/redevelop arcades between Dorset Road/Dorset Sq with adequate lighting, and active frontages. Consolidate sites to ensure minimum store dimensions accommodate sustainable retail tenancies. Provide minimum floor space areas of 300m² per tenancy.
- 7 Investigate the provision of a community bus service to surrounding residential areas (particularly for elderly) providing direct access to Dorset Square.
- 8 Clearly define and prioritise pedestrian movements. Provide directional signs to adjoining areas of interest. Revise current parking layout to facilitate improved circulation and access to bays (wider spaces and aisles to Aust. Standards). Option of 60° angle parks to Australian Standards provides approximately a total supply of 321 car parking spaces (currently 410 car parking spaces).
- 9 Children's multi-use play facility.
- 10 Active edge and provide 24-hour access to the station/Dorset Square at all times of the day and night along the "Principal Pedestrian Pathway".
- 11 Bike rack/storage facilities.
- 12 Relocate existing toilet facility and provide pedestrian scale of lighting in car park areas.

- 13 Direct vehicular access to upper level retail redevelopment of Boronia Mall.
- 14 Investigate the replacement of the compactor with recycle bins. Locate bins with a minimal visual presence, away from active retail edge.
- 15 Formal raised pedestrian crossing to provide central pedestrian access through Dorset Square. Consider design which will provide protection from the elements. Remove/relocate poles/lights that obstruct the direct path of travel.
- 16 Provide public art display space to showcase local artists/youth art.
- 17 Incorporate best practice water sensitive urban design in the layout of parking areas.
- 18 Work with traders & property owners to improve access to commercial buildings.
- 19 Provide lift to upper level car park area. Ensure visibility between the ramp at grade and deck car park areas are maximised. Define/mark pedestrian pathways between Dorset Square and Floriston Road.
- 20 Provide prominent directional signage to car park points of entry.
- 21 Improve lighting and surfaces within deck structure. Incorporate public art/detailing along defined pedestrian pathways. Relocate specialty tenancies to open northern edge of car park (potential to incorporate west of Coles/Kmart). Improve levels of lighting, appearance and amenity. Investigate (at lower levels) the provision of windows through to adjoining retail tenancies (Coles/Kmart).
- 22 Outside of peak times encourage the temporal use of the car park for other activities. Use bollards to direct/restrict traffic providing wider spaces for on-street activity (extension of activity around the cinema), markets and informal recreation.
- 23 New single use toilet facility. Best practice ESD practices in the design of a new toilet facility. Incorporate educational boards to explain environmental aspects. Maximise opportunities for water runoff (from Dorset Square and adjoining buildings, structures), and solar access.
- 24 Investigate the provision of an upper level drop-in youth centre with an outlook over Dorset Square.
- 25 Underground power lines and undertake avenue planting along Floriston Road and Chandler Avenue ring road.
- 26 Provide a direct pedestrian link between Elliot Street, Floriston Road and Dorset Square.

Figure 14(b)

boronia *Your Place, Your Life, Our Future*

Precinct Plan 5(b) - Dorset Square Angle Car Park option

(October 06)

Approximately 285 car parking spaces





Incorporate water sensitive urban design within the revised car park layout.

the car park will clearly define and prioritise pedestrian movement. A formal raised pedestrian crossing will facilitate central pedestrian access through Dorset Square. The design of the walkway should provide protection from the elements and should allow for the removal or relocation of poles that obstruct the direct path of travel.

The revised car park layout and existing change in grade in Dorset Square provide opportunity to incorporate water sensitive urban design principles to treat stormwater runoff prior to discharge.

A review including thorough consultation and discussion of the layout of the Dorset Square car park area is required prior to detailed design and construction. Options include a 60° and 90° angle option.

- Projected growth in expenditure⁴ and an option to attract a wider segment of Boronia's retail catchment is achievable with the redevelopment of the Mall (providing bulky goods, homewares and ancillary stores). The accommodation of a new bulky goods attractor and ancillary household goods stores (refer Figure 14(a) i – Dorset Square Upper Level) on the upper level of the Mall has been identified as a potential opportunity to consolidate core retail activities around Dorset Square and to provide shared economic benefit through linked trips within the mall and surrounding areas.



Agreement must be sought from existing owners, who may be involved in the ownership of new stores. This opportunity provides the potential to invigorate and revitalise Dorset Square as a favoured shopping destination for the local community.

The future redevelopment of the Mall should ensure the design addresses Disability Discrimination Act requirements.

The design of the structure must ensure heritage aspects of the mall (including the tiled mural) are protected and featured in future buildings and works. The design must include an active edge onto Dorset Square.

- In conjunction with the mall owners or through the redevelopment of the mall, undertake improvements to the deck car park structure, improving access, levels of lighting, surface treatment, grade of ramps, ongoing flooding concerns, and seek the installation of security camera's where appropriate. Incorporate public art, *light boxes* and detailing along defined pedestrian pathways. Relocate specialty tenancies to open up the northern edge of car park. Investigate the provision of windows through to adjoining retail tenancies (currently Coles) at lower levels adjacent to pedestrian ramps. The redevelopment of the Mall will incorporate a pram and wheel chair accessible lift to provide direct access to Dorset Square from upper level development and into the upper level car park area. A defined pedestrian pathway through the upper level car park will provide direct grade access to Floriston Road (with a link to Allandale Road). The design of works to the structure should ensure visibility between deck car park and the lifts are maximised.



Light boxes & illuminated signs can be used to integrate lighting, signage and artwork improving perceptions of place & safety during evenings.

⁴ Based on the 'Boronia Commercial & Retail Needs Assessment' Ratio Consultants October 2005.

- Remove the compactor and toilet facilities along the Principal Pedestrian Path in the middle of Dorset Square. Seek locations for a daily pick up of recycle bin facilities behind the building line of development in the Square, or allocate and screen areas within the at grade car park. Two new recycle facilities should be provided north and south of the square to ensure the use of the facilities by retailers.



The Olinda toilet block provides an example of a project which began as atypical toilet block and has been transformed through its design into a community icon reflecting the towns history in timber logging during the 1800's.

- Relocate the toilet and provide a new single use facility (two pans) in the vicinity of the southern section of the Square. Ensure the toilet follows leading edge ESD⁵ principles, by using water runoff and solar panels. Ensure the design of the facility addresses CPTED⁶ principles, and provides a high level of amenity and urban design quality within the square. Incorporate educational boards to explain environmental achievements of this facility.
- Investigate the potential for a regular market in Dorset Square. Consideration should be given to ensure the hours of operation best ensure economic benefit to established retailers within Dorset Square and market operators, and to traffic and transport concerns.
- Establish a working relationship with traders and property owners to improve access in older style commercial buildings and along pedestrian pathways adjacent to store frontages.
- Where necessary, protect pedestrian space through bollards to enable on-street dining protected from vehicle overrun of the pavement.
- Work in partnership with property owners to refurbish or redevelop arcades between Dorset Road and Dorset Square, with adequate lighting and active frontages. Work with owners to consolidate sites to ensure minimum store dimensions accommodate sustainable retail tenancies, providing minimum floor space areas of 300m² per tenancy.
- Negotiate with adjoining property owners for the provision of 24-hour access to the Station and Dorset Square at all times of the day and night along the Principal Pedestrian Pathway.
- Outside of peak times encourage the use of the car park for other activities. Use bollards to direct or restrict traffic, providing wider spaces for on-street activity (extension of activity around the cinema), markets and informal recreation.
- In partnership with community groups and clubs, investigate the provision of an upper level youth centre with an outlook over Dorset Square.
- Investigate underground power lines and undertake avenue planting along Floriston and Chandler Roads.
- Seek a direct pedestrian link between Elliot Street, Floriston Road and Dorset Square.

⁵ Ecologically Sustainable Design.

⁶ Crime Prevention Through Environmental Design

Colours & Materials Schedule: Dorset Square

To the north and the east, the large Mall building and supermarket wraps around Dorset Square. Being located at the foot of the hills, in the landscape view, charcoal colours such as 'Ironstone' should be used on façades with highlights of concrete tonings to the floating panels and blockwork.

A feature of recycled redgum timber slats will highlight the entry points to the mall – both in ceiling and pillars. This use of natural timber aims to warm up entry points for pedestrians and to link to the inside artwork features of the mall – the earthy orange and purple tiled mosaic.

The existing 'Priceline' façade and small shops / arcades to the west side of the square will blend with this palette.

Along the edges of the Square encourage outdoor café tables and screening with 'red earth' umbrellas.

The cinema red brickwork should be retained to aid the 'natural laneway' background particularly along the proposed lightbox walk. This will reconnect the natural based finishes. This space can easily accommodate a mix of old and new finishes – with additions of stainless steel, aluminium cladding, and neon signage amongst old red bricks, bluestone and concrete.



Wattyl 'Ironstone'
Mall & Coles main facade paint

Wattyl 'Slate'
Accent to concrete panels &
blockwork

Victorian River Redgum
Slatted timber to entry of mall eaves

Colourbond 'Dune'
Roofing metal 1

Colourbond 'Ironstone'
Roofing metal 2

Colourbond 'Night Sky'
Roofing metal 3

Dorset Square **5**

6

6.6 Boronia Park

Context

Boronia Park is a fundamental recreational and open space resource adjacent to the commercial environs. During 2004 the Boronia Park Master Plan was prepared by Council in consultation with the local community. This master plan has been incorporated into the Boronia Structure Plan, as previously adopted by Council (with minor amendments to ensure connectivity to the adjoining commercial environments).

The park includes and is adjoined by numerous facilities including:

- the Knox Basketball Stadium
- the Park Crescent Children's Centre
- the Boronia Branch Library
- St Johns Ambulance
- Knox Infolink
- the Progress Hall
- the Boronia Radio Controlled Car Club

Although historically these community and recreational facilities have been collocated around the interface of Boronia Park and the commercial environs, the Boronia Park precinct lacks cohesion and a strong visual presence within the centre. The park and adjoining commercial areas have poor pedestrian connections to adjoining commercial precincts north of Boronia Road. Currently pedestrian access into the precinct is indirect with limited pedestrian crossing north-south across Boronia Road.



*The buildings along the south-west corner of the intersection of Boronia & Dorset Roads are characterised as a **chamfered** building edge. This refers to the curved or rounded nature of buildings fronting this intersection.*

The basketball stadium has a substantial car park, although this facility is underutilised during weekdays. The commercial development which adjoins the car park along the park's northern boundary provides no address or reference to the park. A substantial opportunity exists to encourage dual store frontages and consolidate rear car park areas of shops to provide a pleasant outlook onto the park.

Unique to this precinct is a series of buildings representative of Boronia's growth as a township around the 1920s and 1930s following the establishment of the railway line and Boronia Railway Station. These buildings include the Boronia Progress Hall and a series of Californian Bungalow style residences. Of further note is the Maternal and Child Health Centre on Boronia Road.

The buildings at the intersection of Boronia and Dorset Roads have a unique *chamfered* edge or built form structure. This intersection appears to contain a number of buildings representative of Boronia's development during the 1920s–1950s, although further review of the heritage significance of the buildings is required.

Our Future

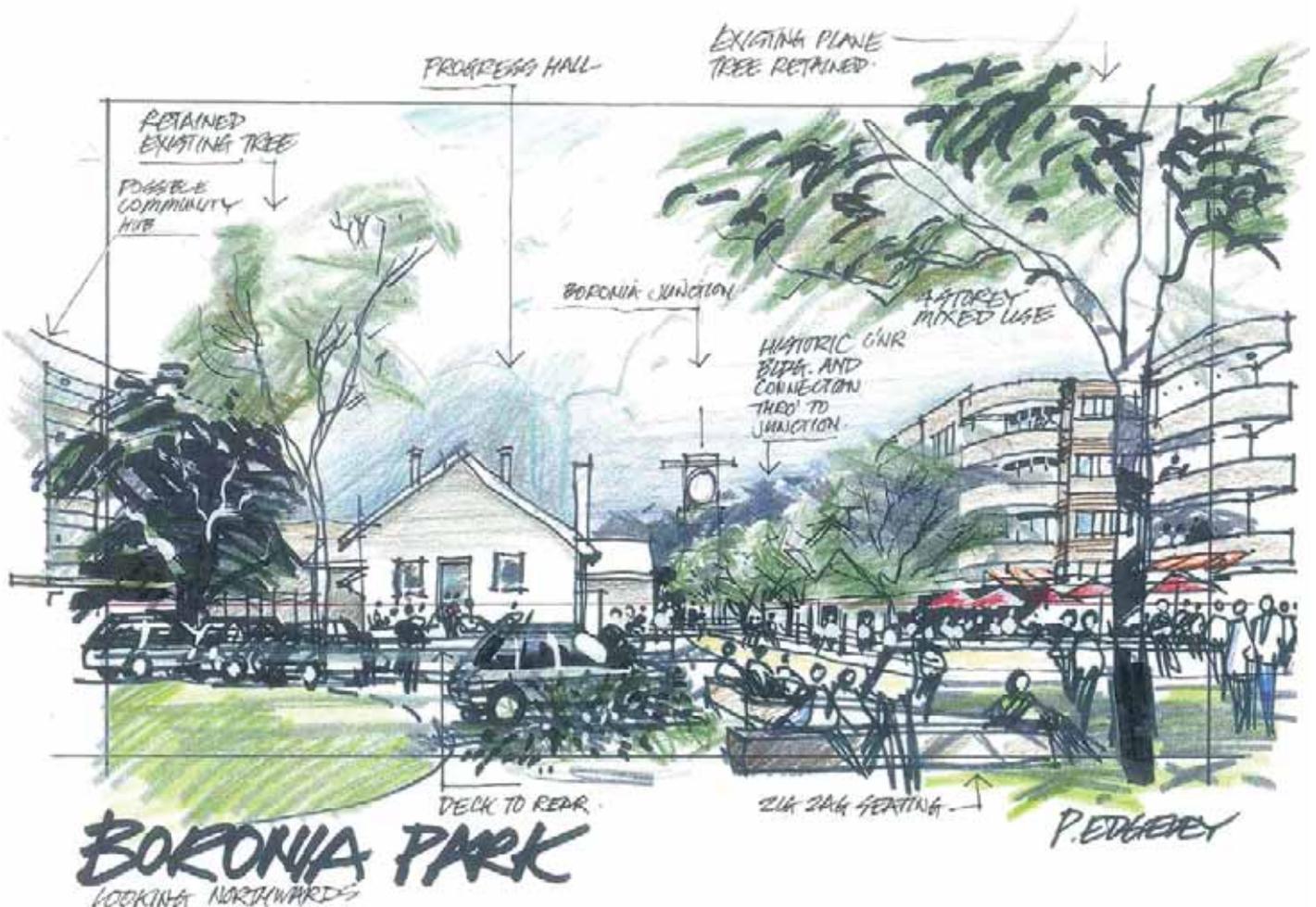
"Boronia Park is a great open space and community hub. The improved landscaping, planting of trees and definition of pedestrian pathways has made Boronia Park much more accessible and a great green space to visit. Progress Hall and some of the buildings around it have been refurbished. The Hall now has a great deck which opens out directly onto the park. Youth groups still meet there but now have a much better outlook.

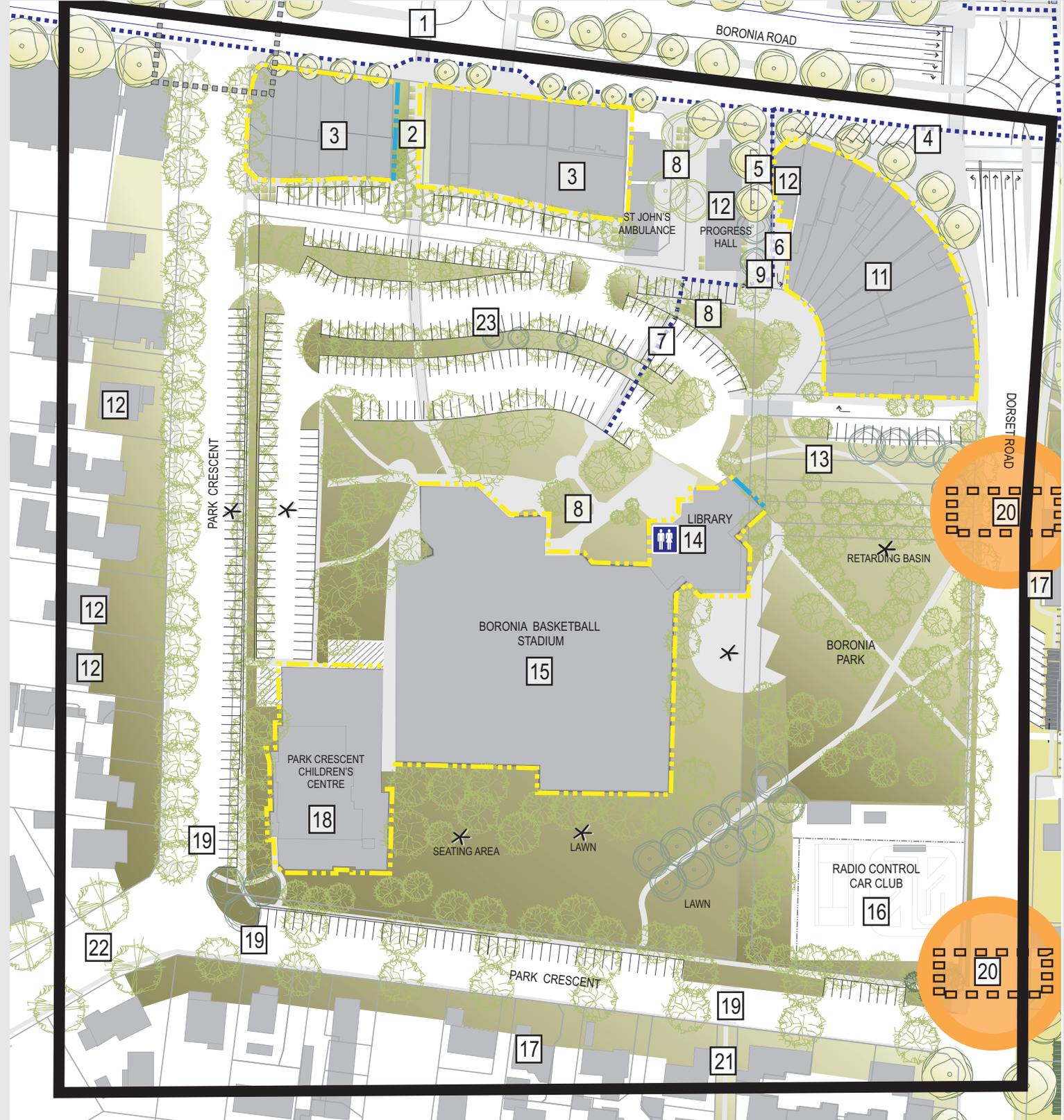
Buildings at the intersection of Boronia and Dorset Roads are four-storeys, and residents have a great outlook from their apartments over Boronia Park and towards the Ranges. In the redevelopment of this area a number of the historic commercial frontages were retained, including the old florist which is now a café that opens out onto the lane and rear courtyard in the corner of the park.

A new road now connects Boronia Park to the commercial area along Erica Avenue. Many of the people that live and work along Erica Avenue now use the park during their lunch breaks. There are many places to sit and read including around the relocated library facility or the BBQ. Many people walk through the park as they complete a lap of the 'Green Link' that now circles the Boronia commercial area.

A new community facility and building has provided a precinct of 'life long learning' on the northern edge of the Park."

View existing car park to landscape works around the Boronia Progress Hall & new infill residential development fronting Boronia Park (existing conditions within inset photo)





- BORONIA PARK PRECINCT AREA
- OPEN SPACE
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- BIKE PATH
- PUBLIC ART / ARCHITECTURAL DETAILING
- ACTIVE / PERMEABLE BUILDING EDGE
- INVESTIGATE FUTURE PEDESTRIAN CROSSING

- TOILET
- 1 Improve connectivity north-south across Boronia Road to improve links between commercial precincts, the community facilities/services precinct and Boronia Park.
- 2 Pedestrian pathway and improved line of sight between areas of interest.
- 3 Investigate new library facility and co-locate facilities to accommodate life long learning and possible community centre. The design of this facility could facilitate an improved pedestrian link north to the commercial environment, whilst improving the visibility and presence of the community facilities/services precinct. Location to be determined.
- 4 Remove slip lane to facilitate pedestrian/bike access across the intersection.
- 5 Extend Boronia Park landscape theme to Boronia Road.
- 6 Encourage active tenancies to provide address to Progress Hall. Enable activity to spill out onto Laneway.
- 7 Direct bike access into the precinct.
- 8 Bike Parking.

- 9 Textured road surface to marcate shared pedestrian/vehicle zone between commercial node and Progress Hall.
- 10 Undertake urban design works to establish a node at the current back-of-shop spaces.
- 11 Mixed-use peripheral commercial/upper level residential development. Provide outlook over Boronia Park and upper level views to the Ranges. Development must include appropriately designed service and utilities areas including rubbish.
- 12 Recognise and protect identifiable heritage values.
- 13 Provide family facilities, playground and BBQ facilities.
- 14 Investigate the relocation of Boronia Library (location to be determined).
- 15 Boronia Basketball Stadium. Provide directional and commercial information for visitors to the stadium to facilitate linked trips.
- 16 Boronia Radio Control Car Club and tennis courts
- 17 Encourage higher density development in heavily landscape setting adjoining and overlooking Boronia Park.
- 18 Investigate the provision of an expanded child care facility, adjacent to a reserve or park (ie Tomore Reserve).
- 19 Pedestrian crossing.
- 20 Dorset Road pedestrian lights to facilitate links to adjoining residential areas. Provide link across railway line for pedestrian access to the rail line bike/walking track.
- 21 Potential green link to Blind Creek. Redevelopment of sites adjoining the path must provide a direct address to the path.
- 22 Avenue/street tree planting marcatng a pedestrian link between Park and Blind Creek.
- 23 Encourage weekday overflow commuter parking.
- Indicates within the adopted Boronia Park Master Plan

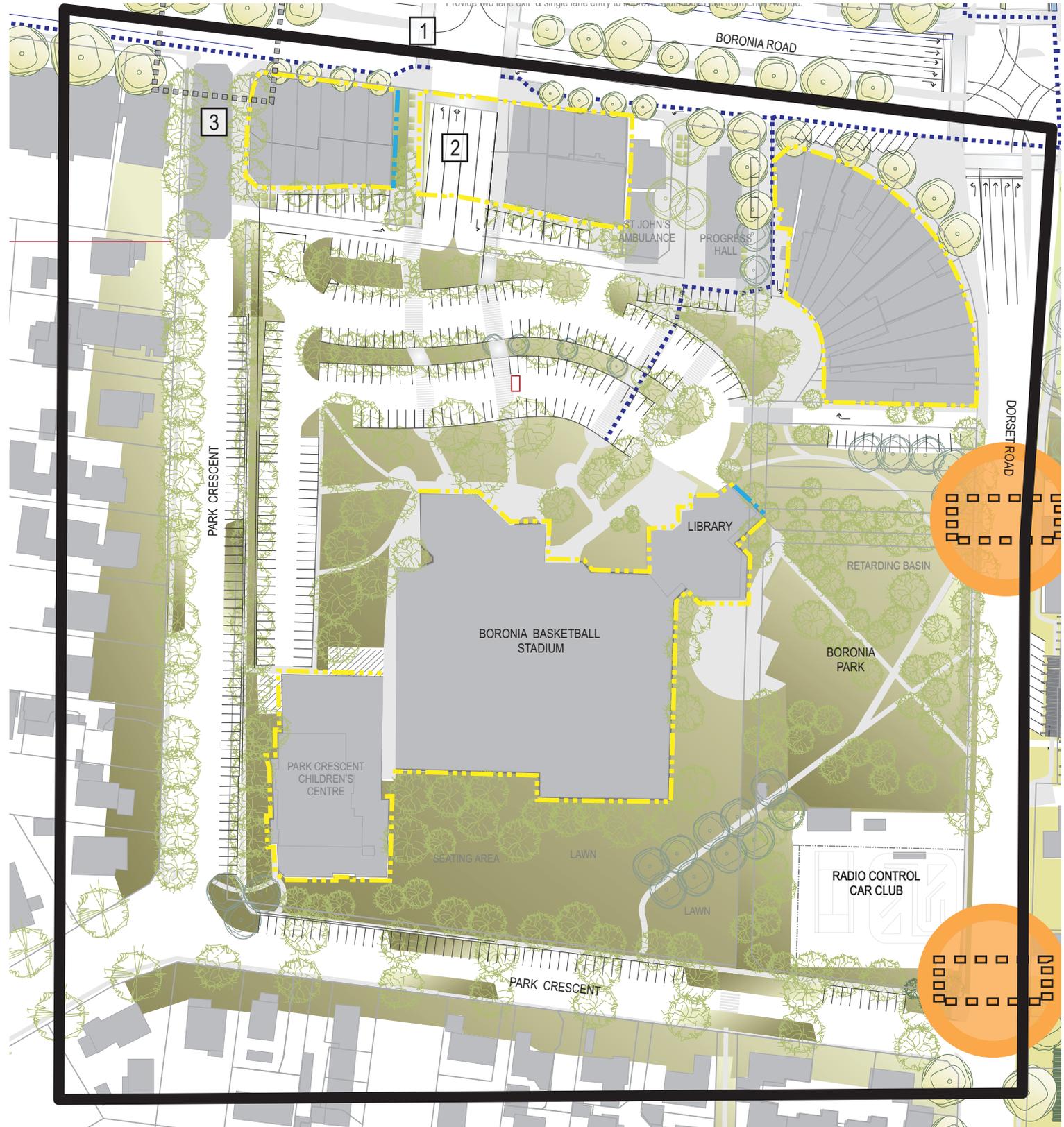
Figure 15

boronia Your Place, Your Life, Our Future

Precinct Plan 6 - Boronia Park

(October 06)





- BORONIA PARK PRECINCT AREA
- OPEN SPACE
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- BIKE PATH
- PUBLIC ART / ARCHITECTURAL DETAILING
- ACTIVE / PERMEABLE BUILDING EDGE
- INVESTIGATE FUTURE PEDESTRIAN CROSSING

- TOILET
- 1** Improve connectivity north-south across Boronia Road to improve links between commercial precincts, the community facilities/services precinct and Boronia Park.
- 2** Investigate alternate vehicular access (Erica Avenue Extension) into Boronia Park from Boronia Road.
- 3** Close Park Crescent to Boronia Road to discourage through traffic.

Figure 15

boronia

Your Place, Your Life, Our Future

Precinct Plan 6(a) - Boronia Park Option 2 (Alternate Access)

(October 06)

Actions and Project Elements

- Establish a Community Hub meeting place on the northern edge of Boronia Park. Undertake a study to investigate the potential to relocate the library as a central point of life long learning in Boronia. Facilities could be provided to house a community centre and citizen groups such as Bellbird Citizens, U3A, CSOs.

The development of this new facility will increase the presence of the Community Hub on the south side of Boronia Road, and will provide the potential for a visually permeable structure linking the commercial environs to the north and the Community Hub precinct around Boronia Park.

- Improve access north-south across Boronia Road to enhance links between Erica Avenue, Boronia Junction and the Community Hub. Acquisition of a commercial site will be required to provide a direct pedestrian and visual link between Boronia Park and Erica Avenue, across a newly constructed pedestrian pathway
- Review the potential to provide (refer Figure 15a) an alternate vehicular access point to the Boronia Park car park from Erica Avenue. This option will require the acquisition of approximately 5 properties. The costs may potentially be offset through the closure of Park Crescent (at the intersection of Boronia Road) and the redevelopment of the roadway. A triple bottom line feasibility analysis should be undertaken in future to assess the benefit of this option. Notwithstanding this option has the potential to address traffic concerns generated by parking & vehicular access at the Boronia Basketball Stadium (and future concerns with the use of this car park for commuter parking) particularly for traffic along Park Crescent.
- Landscape elements within Boronia Park should extend along the new pedestrian link (Erica Avenue link), and around Progress Hall to Boronia Road.
- Work with property owners and tenancies to encourage activity to spill out onto laneway and provide an address to Progress Hall. Facilitate this activity by undertaking urban design works in the lane to establish a node at the current back-of-shop spaces.
- Construct a bike path from Boronia Road to the Basketball stadium and library facility, and provide covered bike parking facilities.
- It is envisaged site consolidation will enable the integrated redevelopment of the chamfered commercial edge in the north-east section of this precinct. A mixed-use peripheral commercial-upper level residential development will provide an outlook over Boronia Park and upper level views to the Ranges. Any planning permit applications on this site must acquire a detailed heritage appraisal of buildings. Development should recognise and protect identifiable heritage values and incorporate these into development design.
- Investigate the need for an expanded child care facility.

-
- Investigate the addition of a pedestrian crossing on Dorset Road (north of Park Crescent) to facilitate links to the higher density residential development between the railway line and Dorset Road. Site acquisition may be required to provide a link across the railway line, to facilitate pedestrian access to the rail bike path and to complete the inner loop of the Green Link.
 - Investigate the potential for a landscaped path along the existing drainage easement to Blind Creek. Redevelopment of sites adjoining the path must provide an address to the path.

Colours & Materials Schedule: Boronia Park

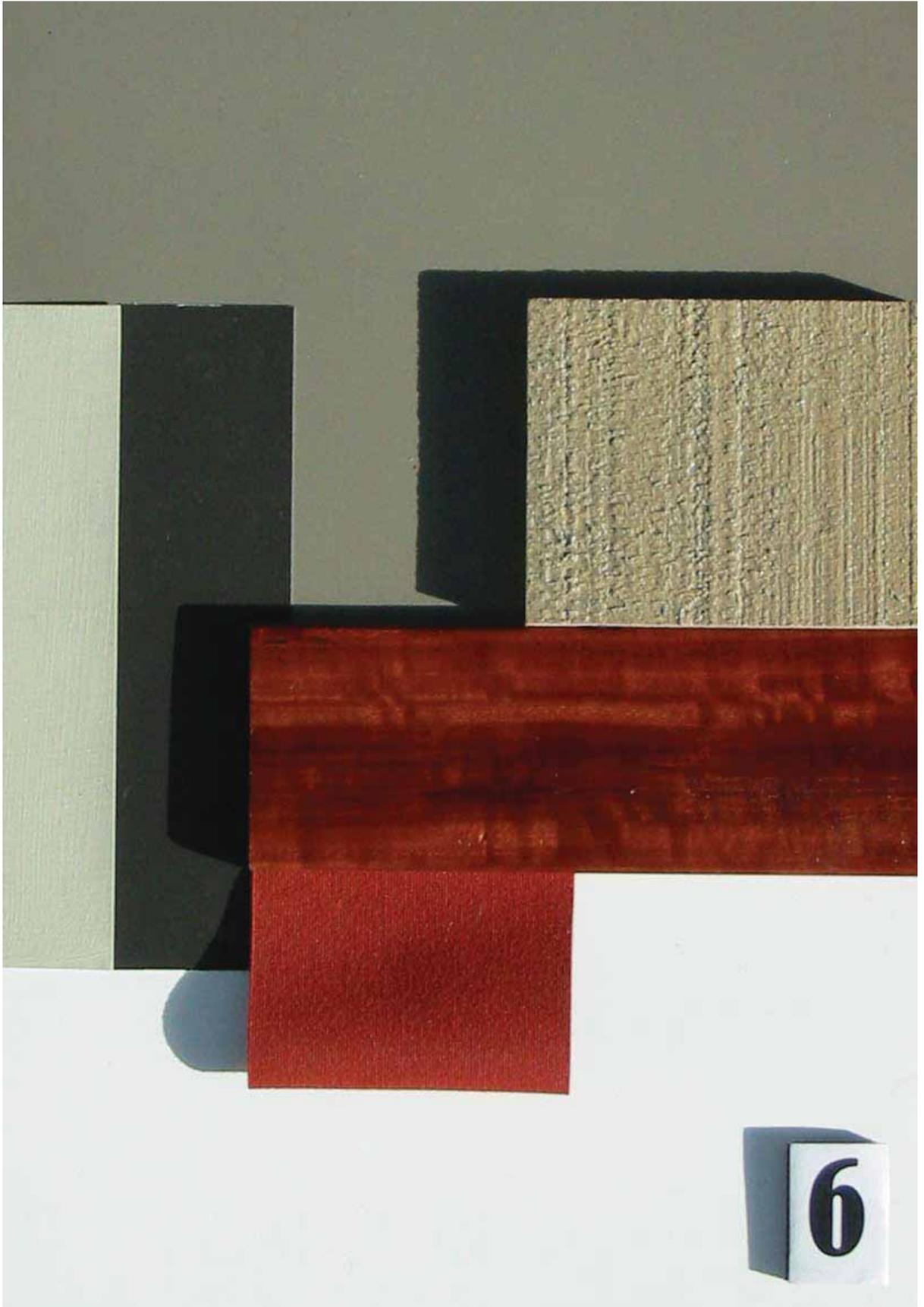
The 1920's small scale California Bungalow buildings such as Progress Hall and the adjoining florist are indicative of the original "bush-town style" and help to build the architectural layers of development within Boronia.

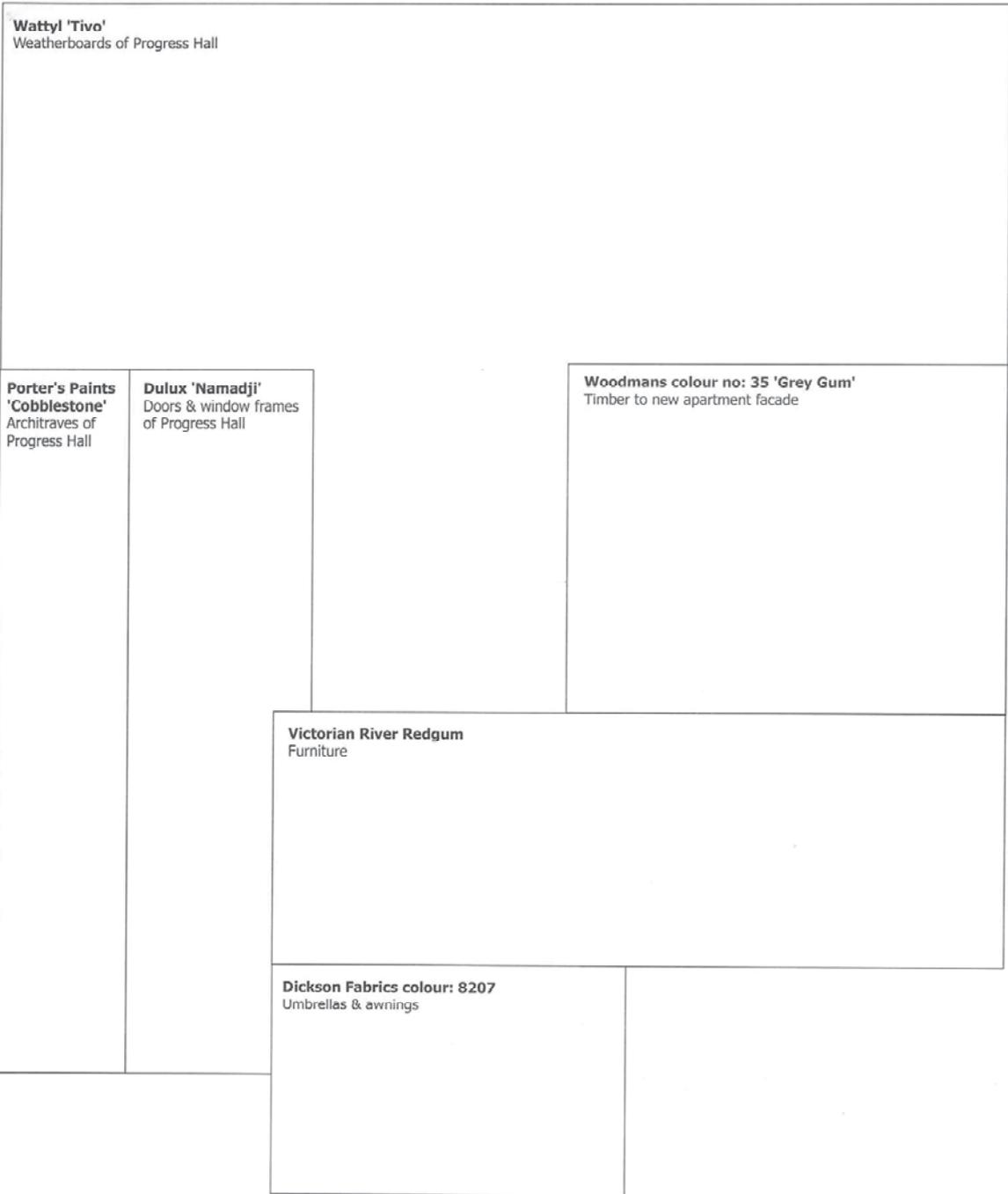
Rather than thinking of new apartment buildings mimicking the old style, a new sophisticated colour scheme could be applied to the older buildings to link to the proposed 'natural visions' for Boronia.

The facade of Progress Hall are a natural brown grey to the weatherboards with light cream punctuating architraves, and charcoal to doors and frames. This colour scheme highlights the 'timber - shed' features while linking to surrounding finishes.

Weathered cedar decking and solid redgum timber furniture expand the timber into the park setting. Highlights of 'red earth' umbrellas provide a warm foreground focus within the greenscape.

These theme's and principals can be readily applied to new development, buildings and works within this precinct.





Boronia Park **6**

7.0 Project Implementation and Prioritisation Plan

The Boronia Structure Plan is based on a series of projects that will require direct public works and investment and which may require strategic intervention (including site consolidations or joint development with the private sector) or policy lead development to attract and support desired outcomes. Figure 16 provides a combination of Council, private sector & State Government projects.

Key projects identified as part of the Boronia Structure Plan (refer to Figure16) fall into several priorities:

- Priority 1 – Catalyst Projects (2005/2006)
- Priority 2 – Foundation Projects (2006/2007)
- Priority 3 - Foundation Projects (2007/2008)
- Priority 4 – Foundation Projects (2008/2009)
- Priority 5 – Foundation Projects (2009/2010)
- Medium-Term (2010–2015)
- Long -Term (2015–2020)

All timelines depend upon appropriate approvals and permits for the relevant authorities and departments.

Catalyst Projects comprise readily achievable public sector works, or combined public-private undertakings. These are projects which will have a high capacity to change perceptions of place and demonstrate a commitment to the betterment of Boronia. Where possible these are projects which will complement and strengthen critical commercial operations.

Foundation Projects are higher priority projects which will considerably improve the broader structure, function and amenity of the Boronia Activity Centre. These are projects which will guide investment within desired locations.

Medium-Term Projects will facilitate the ongoing improvement of the Activity Centre.

Long-Term Projects present as a series of ongoing development and infrastructure opportunities and works within the centre.

Catalyst Projects

Five projects are proposed to commence during the 2005–2006 financial year.

These projects have been selected as they:

- are readily achievable projects that have the capacity to change perceptions;
- will demonstrate a commitment to the betterment of Boronia;
- seek to build community capacity;
- will strengthen newly established commercial operations within the centre.

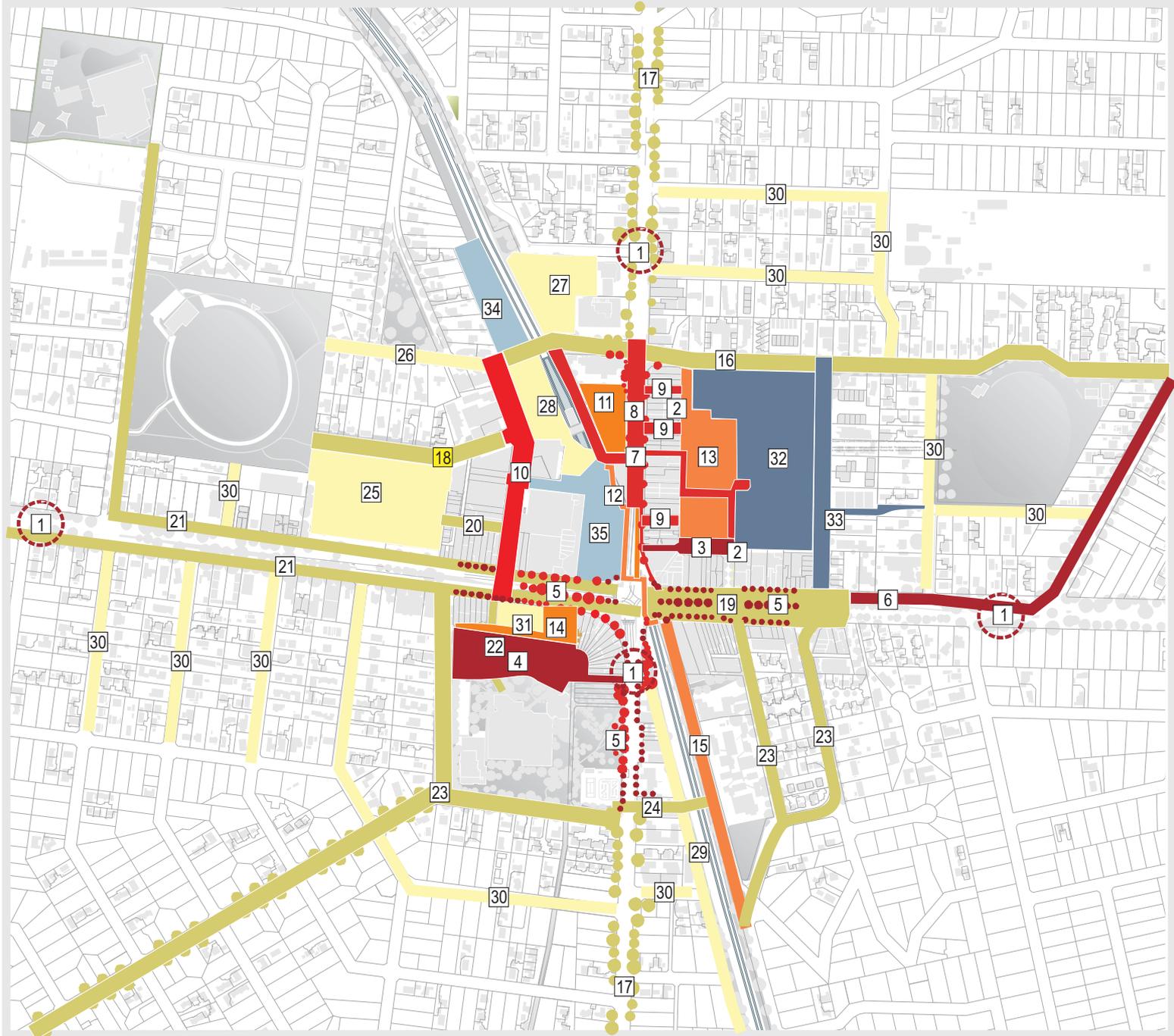
The principal catalyst projects include:

- construction of new footpaths, architectural lighting, seating, and a makeshift public square at Cinema Lane and Dorset Square South (refer to Figure 17 – Project Plan – Dorset Square South and Cinema Lane);
- establishment of the Boronia Business Improvement Program;
- provision of Centre Gateway Design and Directional Signage on approaches and between areas of interest;
- the Relocation of the Compactor and Toilet Facility from centre of Dorset Square;
- contribution to ongoing works as part of the Boronia Park Master Plan.

Funding

The implementation of projects within the Structure Plan will be funded by a number of sources, which may include:

- Council's commitment in its Place Management budget of approximately \$5 million over a five-year period. This will fund infrastructure, public works and programs. Other actions will also be implemented across Council through the Capital Works Program and community development initiatives or in conjunction with the implementation of other plans;



PRIORITY 1 : CATALYST PROJECTS 2005 / 2006

- 1 CENTRE GATEWAY DESIGN AND DIRECTIONAL SIGNAGE *c*
- 2 COMPACTOR AND TOILET RELOCATION *c*
- 3 CINEMA LANE AND DORSET SQUARE SOUTH *c*
- 4 BORONIA PARK: CONTRIBUTION TO WORKS *c*
- 5 STREET TREE PLANTING *c*
- 6 BIKE PATH: BORONIA - THE BASIN *s/c*
BORONIA BUSINESS IMPROVEMENT PROGRAM *c*

PRIORITY 2 : FOUNDATION 2006 / 2007

- 7 EAST WEST CONNECTION: STATION TO DORSET SQUARE *c*
- 8 DORSET ROAD: STREETSCAPE IMPROVEMENTS *c*
- 9 ARCADE IMPROVEMENTS *p*
- 10 ERICA AVENUE: STREETSCAPE WORKS *c*

PRIORITY 3 : FOUNDATION 2007 / 2008

- 11 MIXED-USE DEVELOPMENT: LUPTON WAY *c/p*
- 12 BIKE PATH: MISSING LINK *c/p*

- 13 DORSET SQUARE CAR PARK *c*
- 14 PROGRESS HALL IMPROVEMENTS AND URBAN DESIGN WORKS *c*
- 15 STREET TREE PLANTING *c*

PRIORITY 4 : FOUNDATION 2008 / 2009

- 16 CHANDLER ROAD: GREEN LINK AND BIKE PATH *c*
- 17 MAIN ROAD APPROACH PLANTING *c*
- 18 ORCHID AVENUE GREEN LINK *c*
- 19 BORONIA ROAD: STREETSCAPE WORKS *c*
- 20 THE VILLAGE/ERICA AVE PEDESTRIAN CONNECTION *c*
- 21 BIKE PATH *c/s*
- 22 ERICA AVENUE/BORONIA PARK PEDESTRIAN CONNECTION *c/p*
- 23 STREET TREE PLANTING (GREEN LINK) *c*
- 24 SOUTHERN DORSET RD/RAILWAY LINE CROSSING *c/s*

PRIORITY 5 : FOUNDATION 2009 / 2010

- 25 BORONIA VILLAGE MIXED-USE REDEVELOPMENT *p*
- 26 GENISTA AVENUE STREETSCAPE WORKS: GREEN LINK *c*

- 27 HIGHER DENSITY RESIDENTIAL DEVELOPMENT *p*
- 28 INTERCHANGE: MIXED-USE DEVELOPMENT *s/p*
- 29 RAILWAY BIODIVERSITY CORRIDOR *c*
- 30 STREET TREE PLANTING *c*
- 31 LIBRARY-COMMUNITY FACILITY DEVELOPMENT *c*

MEDIUM TERM : 2010 - 2015

- 32 BORONIA MALL REDEVELOPMENT *p*
- 33 FLORISTON ROAD STREETSCAPE WORKS AND PEDESTRIAN CONNECTION TO ALLANDALE ROAD *c/p*

LONG TERM : 2015 - 2020

- 34 MIXED USE RESIDENTIAL-COMMERCIAL DEVELOPMENT *s/p*
- 35 BORONIA JUNCTION CAR PARK IMPROVEMENTS *p*

RESPONSIBLE PARTIES

- c* - COUNCIL
- s* - STATE GOVERNMENT
- p* - PRIVATE

Note: All time lines dependant on programme budgeting, appropriate approvals and consent of property owners where required.

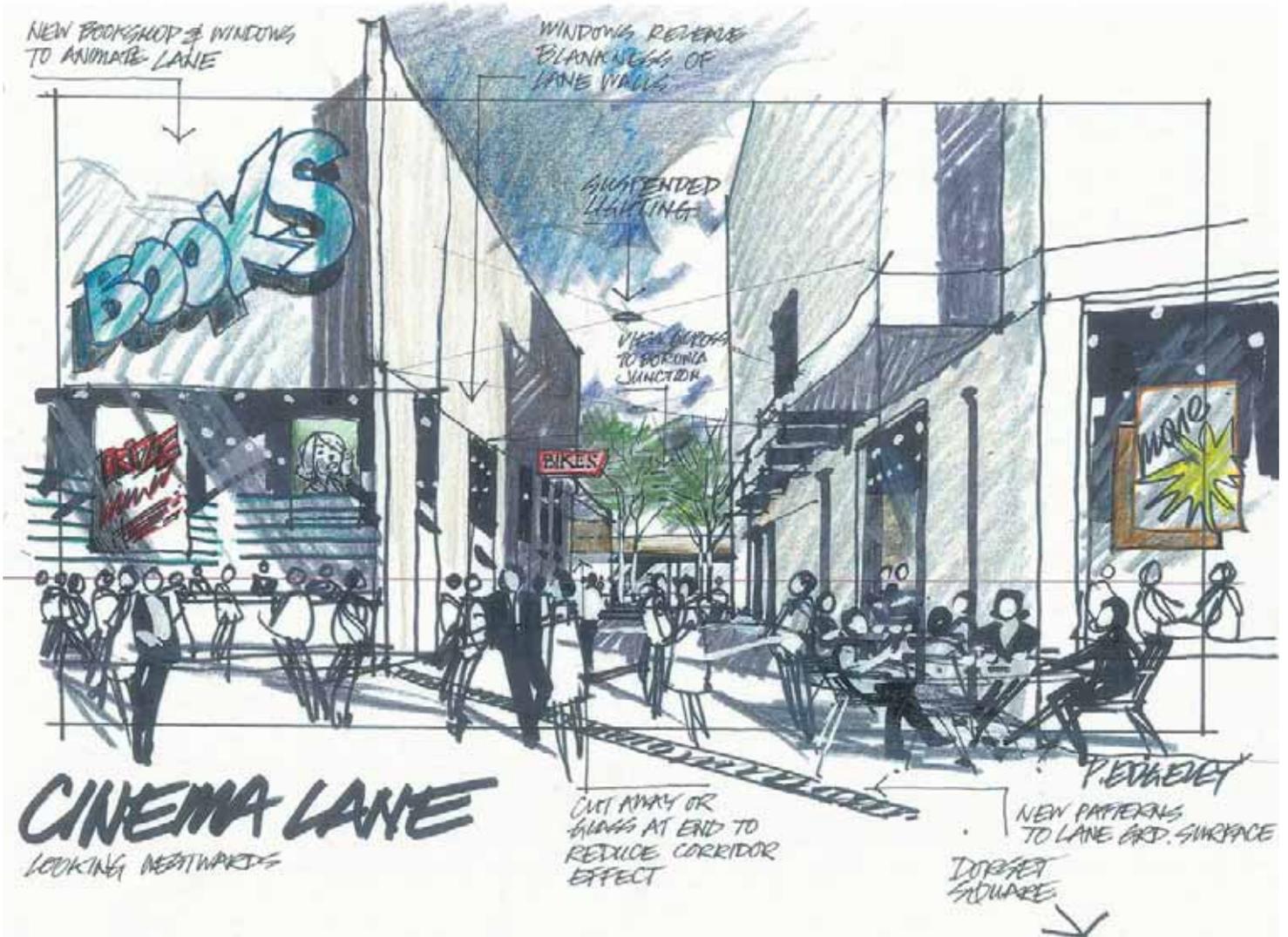


-
- the Federal and State Government through grants to fund urban design and infrastructure improvements or community development projects;
 - Vic Roads to fund pedestrian crossings, road works and bike paths;
 - VicTrack in the rationalisation and potential redevelopment of redundant sites;
 - the establishment of a framework for development contributions, particularly for new infrastructure works, to be obtained from private sector developments. The framework should investigate opportunities to obtain contributions from infill development proximate to the Activity Centre.

Incorporation into the Knox Planning Scheme

The preparation of an amendment to incorporate the recommendations of the Boronia Structure Plan into the Knox Planning Scheme will be prepared in consultation with the Department of Sustainability and Environment to ensure Ministerial Approval.

The amendment may comprise: changes to the Municipal Strategic Statement, the incorporation of the overriding strategic vision and series of precinct-specific performance-based guidelines and objectives within the Local Planning Policy Framework, building height controls and guidelines to direct building form within a design and development overlay, and the incorporation of identified sites within the Heritage Overlay.



Cinema Lane will provide a highly active pedestrian focused evening environment within Boria (existing conditions within inset photo)



Boronia



This report is also available in large print and electronic formats on request from Council's City Strategy Department

Knox City Council

511 Burwood Highway,
Wantirna South 3152 Australia
Telephone: 03 9298 8000 Fax: 039800 3096
Hearing Assisted (TTY): 039298 8521
Upon request information in this document can be
made available in braille and audio formats.
Email:
knoxcc@knox.vic.gov.au
www.knox.vic.gov.au